

GENERAL REQUIREMENTS:

1A. GENERAL:
THESE DRAWINGS AND SPECIFICATIONS ARE WRITTEN TO DESIGNATE CERTAIN PRODUCTS AND METHODS OF EXECUTION BUT ARE NOT INTENDED TO BE COMPLETE AND COVER ALL ITEMS OF CONSTRUCTION. THE CONTRACTOR SHALL USE REGIONALLY RECOGNIZED STANDARDS AND PROCEDURES IN HIS CONSTRUCTION UNLESS OTHERWISE SPECIFICALLY CALLED FOR. IN CASE OF QUESTION, NOTIFY ARCHITECT.

1B. ERRORS AND OMISSIONS:
THE CONTRACTOR SHALL NOT TAKE ADVANTAGE OF ANY UNINTENTIONAL ERROR OR OMISSION IN THE DRAWING OR SPECIFICATIONS. THE CONTRACTOR SHALL BE EXPECTED TO FURNISH ALL NECESSARY MATERIAL AND LABOR THAT ARE NECESSARY TO MAKE A COMPLETE JOB TO THE TRUE INTENT OF THE CONSTRUCTION DOCUMENTS. THE CONTRACTOR IS RESPONSIBLE TO BRING ANY DISCREPANCIES TO THE ATTENTION OF THE ARCHITECT IN WRITING.

1C. DIMENSIONS AND SCALE:
THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING EXISTING CONDITIONS, DIMENSIONS AND ELEVATIONS PRIOR TO START OF CONSTRUCTION. WRITTEN DIMENSIONS ARE TO TAKE PRECEDENCE OVER SCALED DIMENSIONS AT ALL TIMES. SIMILAR DETAILS SHALL APPLY AT SIMILAR CONDITIONS. LARGE SCALE DRAWINGS ARE TO TAKE PRECEDENCE OVER SMALLER SCALE DRAWINGS. NO DEVIATIONS FROM THE DRAWINGS SHALL BE MADE EXCEPT WITH THE CONSENT OF THE ARCHITECT.

1D. CONSTRUCTION OBSERVATION/ REVIEWS DEFINITION:
CONSTRUCTION OBSERVATION AS USED IN THESE SPECIFICATIONS MEAN VISUAL OBSERVATION OF MATERIALS, EQUIPMENT AND CONSTRUCTION WORK ON AN INTERMITTENT BASIS TO DETERMINE THAT THE WORK IS IN SUBSTANTIAL CONFORMANCE WITH THE CONTRACT DOCUMENTS AND ITS DESIGN INTENT. SUCH OBSERVATION DOES NOT CONSTITUTE ACCEPTANCE OF THE WORK NOR SHALL BE CONSTRUED TO RELIEVE THE CONTRACTOR IN ANY WAY OF HIS RESPONSIBILITY TO PROVIDE AND INSTALL ITEMS AS CALLED OUT IN THE DRAWINGS AND SPECIFICATIONS.

1E. GUARANTEE:
ALL LABOR, MATERIAL AND EQUIPMENT SHALL BE GUARANTEED FOR A MINIMUM PERIOD OF (1) ONE YEAR FROM DATE OF ACCEPTANCE AGAINST DEFECTS IN WORKMANSHIP AND/OR MATERIALS, UNLESS OTHERWISE STATED OR NOTED, WHERE THE STATUTE OF LIMITATION APPLIES, IT SHALL TAKE PRECEDENCE.

1F. CLEAN - UP:
CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUS REMOVAL AND DISPOSAL OF ALL CONSTRUCTION DEBRIS. ALL ITEMS SHALL BE LEFT FREE FROM ALL FOREIGN MATTER. AT COMPLETION, HOUSE SHALL BE PROFESSIONALLY CLEANED.

GENERAL NOTES:

1). FINISHED GROUND SURFACES SHALL BE GRADED TO DRAINAGE THE FINISHED SITE PROPERLY. FINISHED GROUND SLOPE WITHIN FIVE FEET OF THE BUILDING OR STRUCTURE SHALL SLOPE AWAY AT A 5%. ALL EXTERIOR HARD SURFACES (INCLUDING TERRACES) SHALL BE INSTALLED WITH A 1% MINIMUM SLOPE AND SHALL DRAIN AWAY FROM THE BUILDING. DRAINAGE SWALES SHALL HAVE A MINIMUM SLOPE OF 1.5%. MAXIMUM ALLOWABLE GRADED SLOPE IS 3 HORIZONTAL TO 1 VERTICAL (33%)

2). LOT GRADING SHALL CONFORM AT THE PROPERTY LINES AND SHALL NOT SLOPE TOWARD PROPERTY LINES IN A MANNER WHICH WOULD CAUSE STORM WATER TO FLOW ONTO NEIGHBORING PROPERTY. HISTORIC DRAINAGE PATTERNS SHALL NOT BE ALTERED IN A MANNER TO CAUSE DRAINAGE PROBLEMS TO NEIGHBORING PROPERTY.

3). NEW RAINWATER DOWNSPOUTS SHALL BE DISCONNECTED AND DIRECT RUNOFF TO A LANDSCAPED AREA. DOWNSPOUTS MAY BE CONNECTED TO A POP-UP DRAINAGE EMITTER IN THE LANDSCAPED AREA OR MAY DRAIN TO SPLASH BLOCKS OR COBBLESTONES THAT DIRECT WATER AWAY FROM THE BUILDING.

4). CONTRACTOR IS RESPONSIBLE FOR DUST CONTROL AND INSURING THE AREA ADJACENT TO THE WORK IS LEFT IN A CLEAN CONDITION.

5). ALL DOWNSPOUTS TO BE RELEASED TO THE GROUND SURFACE, DIRECTED AWAY FROM BUILDING FOUNDATIONS AND DIRECTED TO LANDSCAPED AREAS.

6). PROVIDE 5% MIN. SLOPES FOR GRADE AWAY FROM FOUNDATIONS AND DRAINAGE AWAY FROM ADJACENT PROPERTY LINES.

ENERGY COMPLIANCE NOTES:

ALL RECESSED LUMINARIES IN INSULATED CEILINGS SHALL BE IC RATED, ELECTRONIC BALLAST AND AIR-TIGHT (AT).

SUPPLEMENTARY CONDITIONS:

1. THE CONTRACTOR SHALL THOROUGHLY EXAMINE THE PREMISES AND SHALL BASE HIS BID UPON EXISTING CONDITIONS. ALL EXISTING CONDITIONS SHALL BE VERIFIED FOR COMPATIBILITY WITH HIS NEW CONSTRUCTION SHOWN HEREIN. IN THE EVENT THAT DISCREPANCIES ARE FOUND IN THE DRAWINGS, THE ARCHITECT SHALL BE NOTIFIED BEFORE WORK CAN PROCEED.

2. THE CONTRACTOR SHALL FIELD INSPECT THE JOB SITE PRIOR TO COMMENCEMENT OF WORK AND SHALL ADHERE ACCESS, SAFETY AND THE USE OF THE FACILITIES AS SET BY THE BUILDING OWNER, BUILDING DEPARTMENT AND ALL OTHER LOCAL AGENCIES.

3. THE CONTRACTOR SHALL PROVIDE ALL WORK AND MATERIAL IN ACCORDANCE WITH ALL FEDERAL, STATE AND LOCAL CODES PER INDUSTRY STANDARDS UNLESS NOTED OTHERWISE. ALL PRODUCTS SHALL BE USED PER MANUFACTURERS RECOMMENDATION.

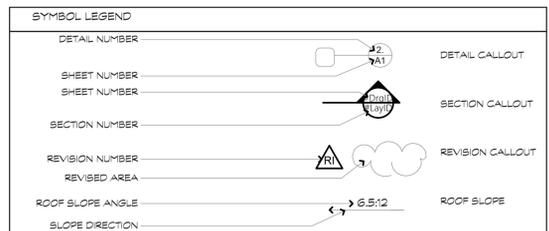
4. ANY PORTION OF THE EXISTING BUILDING, SITE OR LANDSCAPING DAMAGED BY THE CONTRACTOR OR HIS SUBCONTRACTORS SHALL BE RESTORED TO THE CONDITION PRIOR TO DAMAGE AT NO COST TO THE OWNER OR TENANT.

5. NEITHER THE ARCHITECTS REVIEW OR APPROVAL OF SHOP DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM THE RESPONSIBILITY FOR DEVIATIONS FROM THE INTENT OF THE DRAWINGS AND SPECIFICATIONS UNLESS AGREED TO BY ARCHITECT IN WRITTEN FORM.

6. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF ALL WORK IN THE FIELD TO ENSURE TIMELY COMPLETION OF THE PROJECT.

7. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR ALL WORK REQUIRED TO COMPLETE THE WORK, INCLUDING, BUT NOT LIMITED TO:

- i) ALL WORK REQUIRED TO PREPARE FOR NEW WORK.
- ii) THE REMOVAL OR RELOCATION OF ALL EXISTING PIPES, CONDUITS, WIRES, ETC. AS REQUIRED IN ORDER TO COMPLETE THE WORK.
- iii) ALL TEMPORARY SHORING REQUIRED DURING CONSTRUCTION TO ALLOW FOR NEW WORK.
- iv) THE PROTECTION OF THE WORK FROM ANY DAMAGE DUE TO FIRE, WEATHER OR VANDALISM.
- v) THE REMOVAL OF ALL DEBRIS AND EXCESS MATERIAL AND BROOM CLEANING EACH AREA AFTER WORK IS COMPLETED.



PROJECT INFORMATION:

OWNER: KUBODERA JOHN
ADDRESS: 216 DEVONSHIRE BLVD. SAN CARLOS, CA 94070 650.742.1721
CONTACT: cyndds@yahoo.com, kubo99@hulamail.com
EMAIL:

DESIGNER: AYESHA SIKANDAR
COMPANY: MA DIMENSIONS
ADDRESS: 533 AIRPORT BLVD., SUITE 220 BURLINGAME, CA 94010 650-373-2166
CONTACT: ayesha@madimensions.com
EMAIL:

STRUCTURAL ENG.: BRIAN MOON
COMPANY: SEZEN & MOON STRUCTURAL ENGINEERING, INC.
ADDRESS: 274 E. HAMILTON AVE. SUITE C CAMPBELL, CA 95008 408.671.7273
CONTACT: SMSEinc@aol.com
EMAIL:

SURVEY ENG.: SAEID RAZAVI, PE
COMPANY: SMP ENGINEERS
ADDRESS: 1534 CAROB LANE, LOS ALTOS, CA. 94024 (650)941-8055

CIVIL ENG.: SAEID RAZAVI, PE
COMPANY: SMP ENGINEERS
ADDRESS: 1534 CAROB LANE, LOS ALTOS, CA. 94024 (650)941-8055

TITLE-24 ENGINEER: TBD
COMPANY:
ADDRESS:

CONTACT: TBD
EMAIL:

CONTRACTOR: TBD
COMPANY:
ADDRESS:
CONTACT:
EMAIL:

CAL GREEN: TBD
COMPANY:
ADDRESS:

EMAIL:
GREEN POINT:

APPLICABLE CODES:

2016 CBC, CPG, CMG AND CEC AS AMENDED BY STATE OF CA AND LOCAL JURISDICTIONS.
2016 CALIFORNIA ENERGY CODE

CALIFORNIA FIRE CODE	2016 EDITION
CALIFORNIA BUILDING CODE	2016 EDITION
CALIFORNIA RESIDENTIAL CODE	2016 EDITION
CALIFORNIA MECHANICAL CODE	2016 EDITION
CALIFORNIA PLUMBING CODE	2016 EDITION
CALIFORNIA ELECTRICAL CODE	2016 EDITION
ENERGY EFFICIENCY STANDARDS (TITLE 24)	2016 EDITION

PROJECT DATA TABLE:

APN #: 049-110-160
ADDRESS: 216 DEVONSHIRE, SAN CARLOS CA.
ZONING: S-71-DR
DISTRICT: DEVONSHIRE
PARCEL SIZE: 11,195 SQ. FT.
OCCUPANCY: R-1
TYPE OF CONST: RESIDENCE
BLOCK ID: 31
LOT NUMBER: 4
(E) LANDSCAPING AREA:

(P) LANDSCAPING AREA:
11,195 -1ST- GARAGE - HARD AREAS
11,195 - (1,815 + 520 + 3,694) = 5,166 SQ.FT
TOTAL
ALLOWED BUILDING COVERAGE: 50%
MAX BSCAR %:
BSCAR % X LOT AREA
(0.5 X 11,195) = 5,597.50

ALLOWED BUILDING FAR:
0.21 (LOT AREA - 5000) + 2000 SQ.FT
0.21 (11,195 - 5000) + 2000 = 3,301
(E) 1ST FLOOR: 2,305 SQ FT
(E) GARAGE: 504 SQ FT
(E) PORCH: 141 SQ FT

(E) BUILDING COVERAGE:
2305,504+141 SQ. FT.: 2950 SQ FT

(E) BUILDING FAR:
2305,104+141 SQ. FT.: 2551 SQ FT

(P) 1ST FLOOR: 1,815 SQ FT
(P) 2ND FLOOR: 1,366 SQ FT
(P) GARAGE: 520 SQ FT
(P) OVERHANG AREAS: 171 SQ FT

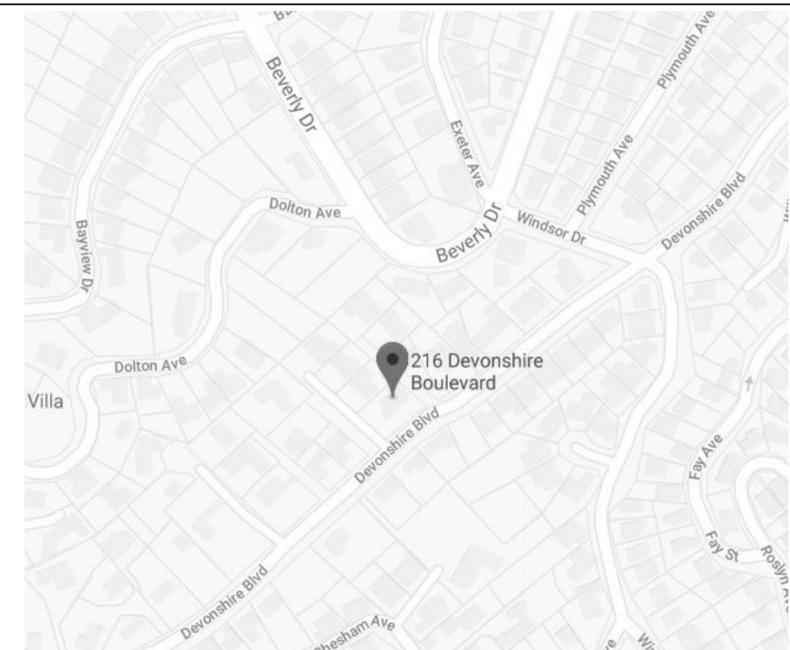
(P) BUILDING COVERAGE:
1,815 +520 +171 SQ. FT.: 2,506 SQ FT
2,506 / 11,195 22%

(P) BUILDING FAR:
1,815 +1,366 + (520-400)=120: 3,301 / 11,195 29.50%

SCOPE OF WORK:

1. ADD NEW 2ND. STORY
2. REDUCTION OF SQ. FT. (E) 1ST. STORY.
3. REMODEL (E) 1ST. STORY.
4. HOME SHALL BE CONNECTED TO EXISTING UTILITIES.

DRAWING INDEX	
ID	NAME
A0.0	COVER SHEET
A1.0	EXISTING/DEMO FLOOR PLAN
A1.1	EXISTING/ DEMO ROOF PLAN
A1.2	EXISTING ELEVATIONS
A1.3	EXISTING SECTIONS
A1.4	EXISTING TREE PLAN
A2.0	(P) SITE PLAN
A2.1	(P) 1ST STORY PLAN
A-2.1	(P) 2ND STORY PLAN
A2.3	(P) ROOF PLAN
A2.4	(P) LANDSCAPE PLAN
A3.0	(P) FRONT ELEVATION
A3.1	(P)ELEVATIONS
A3.2	(P) ELEVATIONS
A4.0	(P) SECTIONS
A4.1	(P) 10 HIDDEN VALLEY SECTION
A5.0	(P) EXTERIOR RCP 1ST STORY
A5.1	(P) RCP 2ND STORY PLAN
A6.0	(P) KEY PLANS
A-7.0	DETAILS
SU-1	SURVEY
C-1	COVER SHEET
C-2	GRADING AND DRAINAGE PLANS
C-3	UTILITY PLAN
C-4	DETAILS
C-5	EROSION CONTROL PLAN
C-6	BEST MANAGEMENT PRACTICES



VICINITY MAP

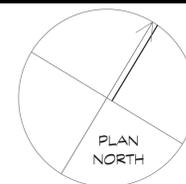
LEGEND

- - - PROPERTY LINE
- E - ELECTRICAL POWER LINE
- S - SEWER LINE
- CW - COLD WATER LINE
- G - GAS LINE
- TPZ TREE PROTECTION ZONE
- Ⓛ DOOR SCHEDULE
- Ⓜ WINDOW SCHEDULE
- RWL RAIN WATER LEADER
- (E) EXISTING
- (N) NEW
- (R) REMODELLED
- UON UNLESS OTHERWISE NOTED
- BO BY OWNER
- OCPI OWNER PROVIDE, CONTRACTOR INSTALL
- VIF VERIFY IN FIELD
- FF FACE OF FINISH
- AFF ABOVE FINISH FLOOR
- TYP TYPICAL



TONG
RESIDENCE

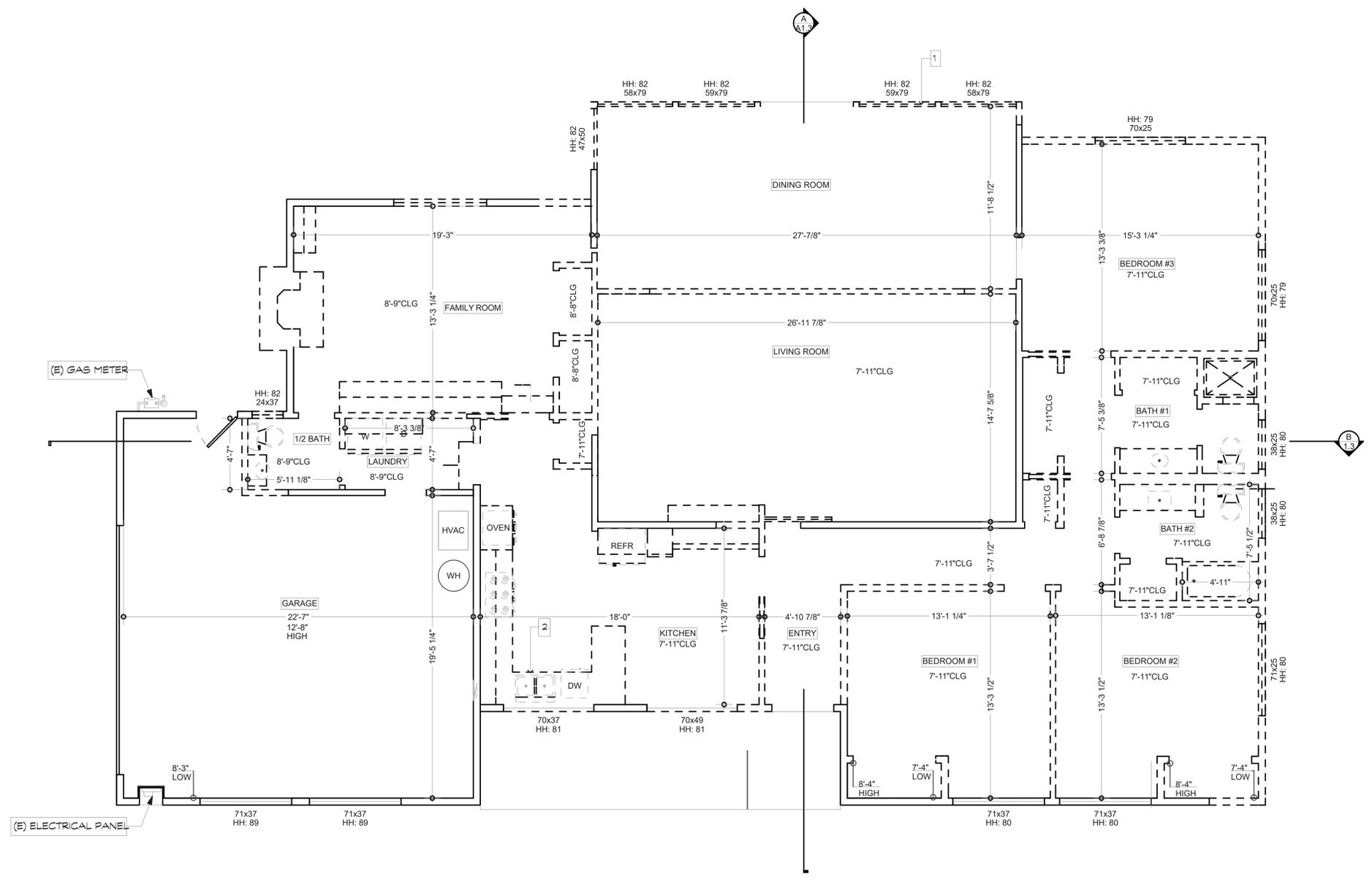
216 DEVONSHIRE BLVD SAN CARLOS, CA
SAN MATEO COUNTY 94070



COVER SHEET

AO.O

LEGEND/ NOTES	
	EXISTING WALLS TO REMAIN
	DEMO WALLS
	REMOVE (E) DOOR/ WINDOWS
	REMOVE (E) FIXTURES

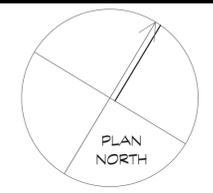


(E) DEMO 1ST FLOOR
SCALE: 1/4" = 1'-0" 1



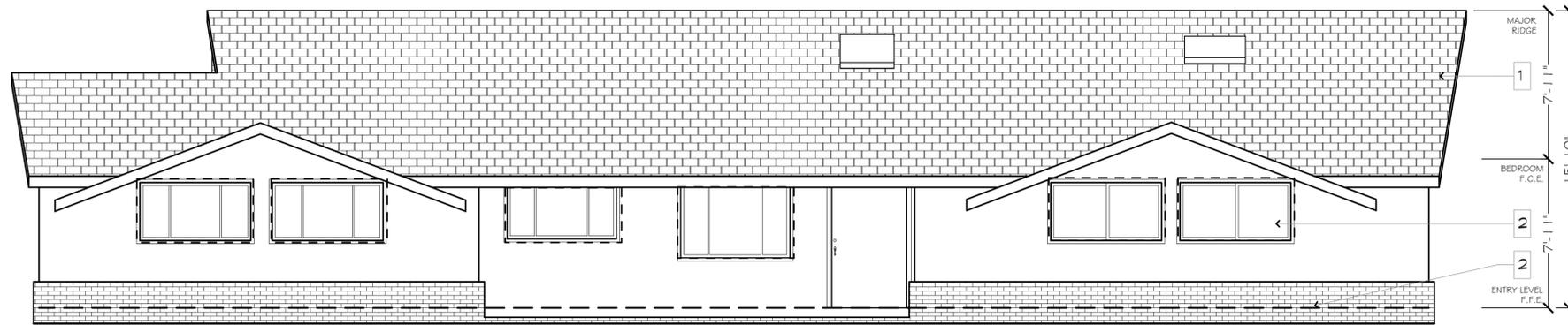
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SAN MATEO COUNTY 94070



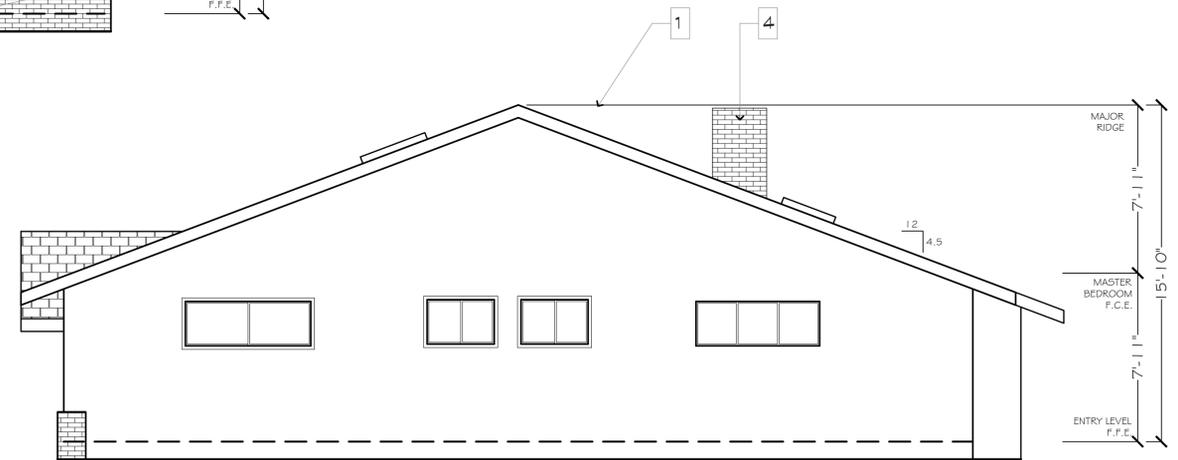
EXISTING/DEMO
FLOOR PLAN

A1.0

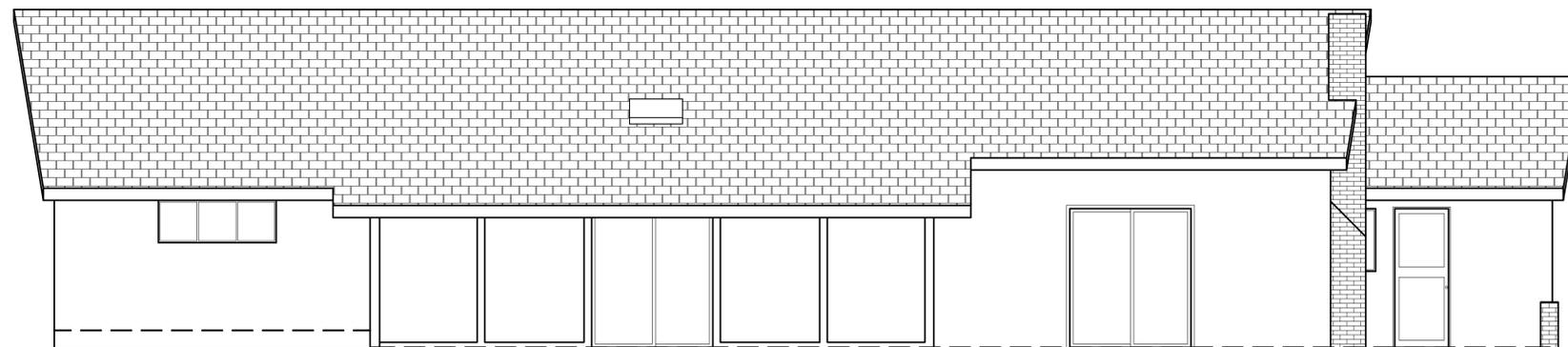


SOUTHEAST
(FRONT ELEVATION)
SCALE" 1/4"=1'-0"

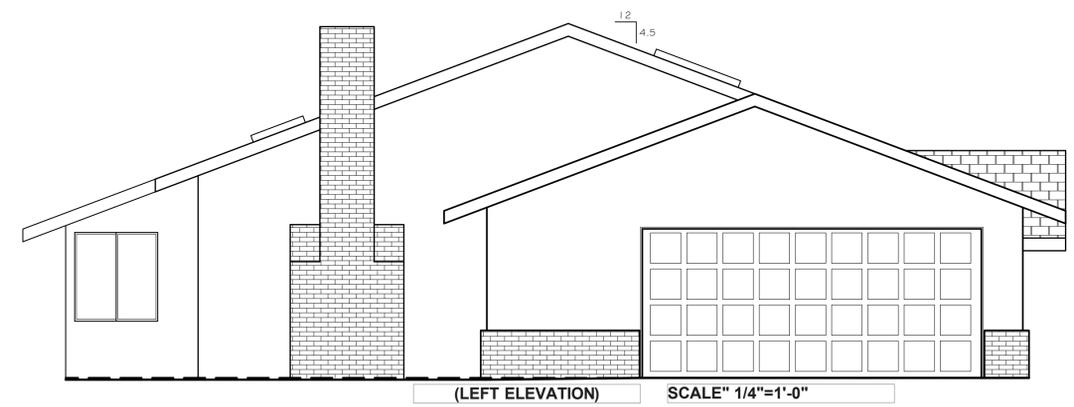
- (E) ELEVATIONS LEGEND/
NOTES
- 1 DEMO (E) ROOF
 - 2 DEMO (E) DOOR/ WINDOWS
 - 3 REMOVE (E) BRICK
 - 4 REMOVE (E) BRICK CHIMNEY



NORTHEAST
(RIGHT ELEVATION)
SCALE" 1/4"=1'-0"



NORTHWEST
(REAR ELEVATION)
SCALE" 1/4"=1'-0"

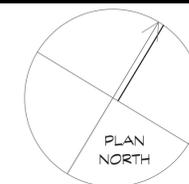


(LEFT ELEVATION) SCALE" 1/4"=1'-0"



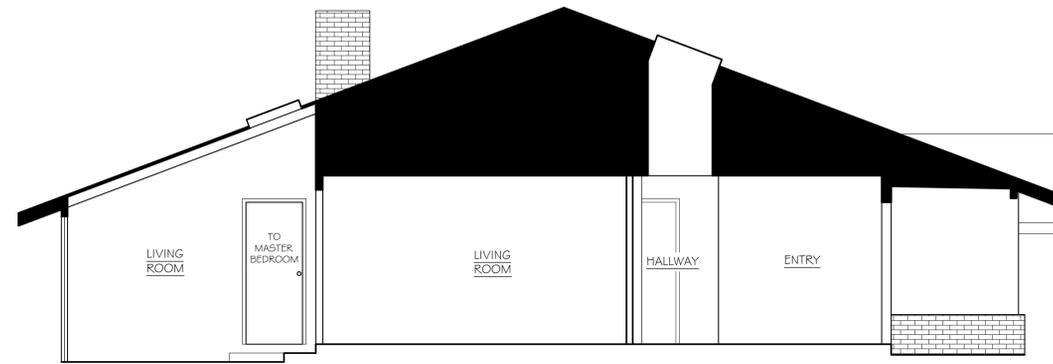
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RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
SAN MATEO COUNTY 94070



EXISTING
ELEVATIONS

A1.2



(A) SECTION
SCALE: 1/4" = 1'-0"

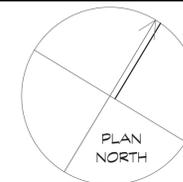


(B) SECTION
SCALE: 1/4" = 1'-0"



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216 DEVONSHIRE BLVD SAN CARLOS, CA
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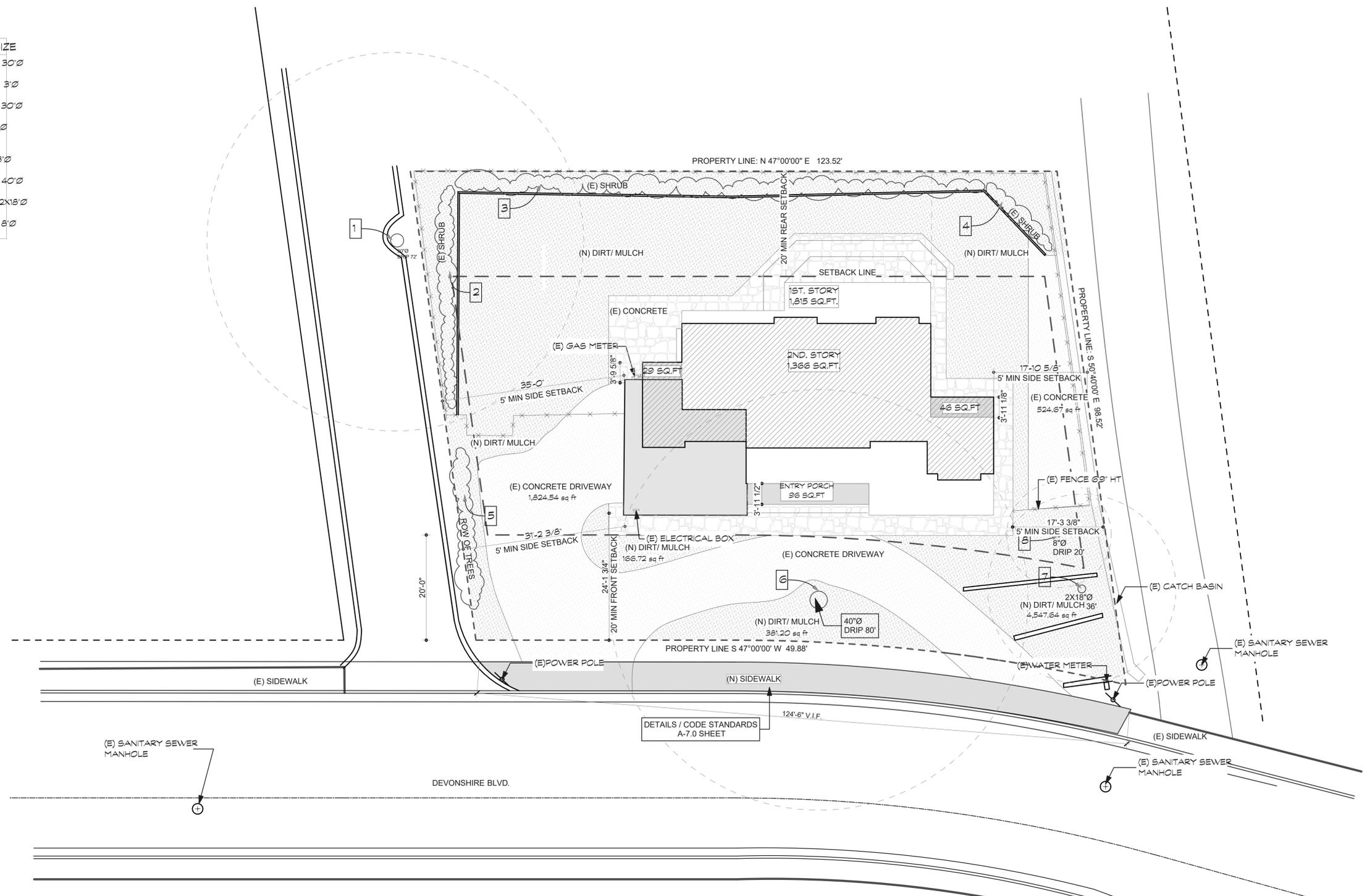


EXISTING
SECTIONS

A1.3

CONDITION	TYPE	DRIPLINE	TRUNK SIZE
1	EXISTING OAK TREE	72'Ø	30"Ø
2	EXISTING IVY / SHRUB OAKS	4' X 43'	3"Ø
3	EXISTING AZALEA AND IVY	4' X 102'	30"Ø
4	EXISTING APPLE TREE / BAMBOO	4' X 18'	3"Ø
5	EXISTING CYPRESS TREES	4' X 30'	3"Ø
6	EXISTING OAK TREE	80'Ø	40"Ø
7	EXISTING OAK TREES	36'Ø	2X18"Ø
8	EXISTING OAK TREE	20'Ø	8"Ø

GENERAL NOTES:
 EXISTING TREES SHALL NOT BE REPLACED OR REMOVED FROM THE ORIGINAL PLACE AS PART OF THIS PERMIT.

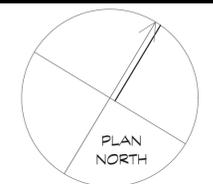


(E) TREE PLAN 1
 SCALE: 1" = 10'



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216 DEVONSHIRE BLVD SAN CARLOS, CA
 SAN MATEO COUNTY 94070



EXISTING TREE
 PLAN

A1.4

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 DISTRICT: DEVONSHIRE
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 OCCUPANCY: R-1
 TYPE OF CONST: RESIDENCE
 BLOCK ID: 31
 LOT NUMBER: 4
 (E) LANDSCAPING AREA:

(P) LANDSCAPING AREA:
 11,195 -1ST- GARAGE - HARD AREAS
 11,195 - (1,815 + 520 + 3,694) = 5,166 SQ.FT
 TOTAL

ALLOWED BUILDING COVERAGE:
 MAX BSCAR %: 50%
 BSCAR % X LOT AREA
 (0.5 X 11,195) = 5,597.50

ALLOWED BUILDING FAR:
 0.21 (LOT AREA - 5000) + 2000 SQ.FT
 0.21 (11,195 - 5000) + 2000 = 3,301

(E) 1ST FLOOR: 2,305 SQ FT
 (E) GARAGE: 504 SQ FT
 (E) PORCH: 141 SQ FT

(E) BUILDING COVERAGE:
 2305,504+141 SQ. FT.: 2950 SQ FT

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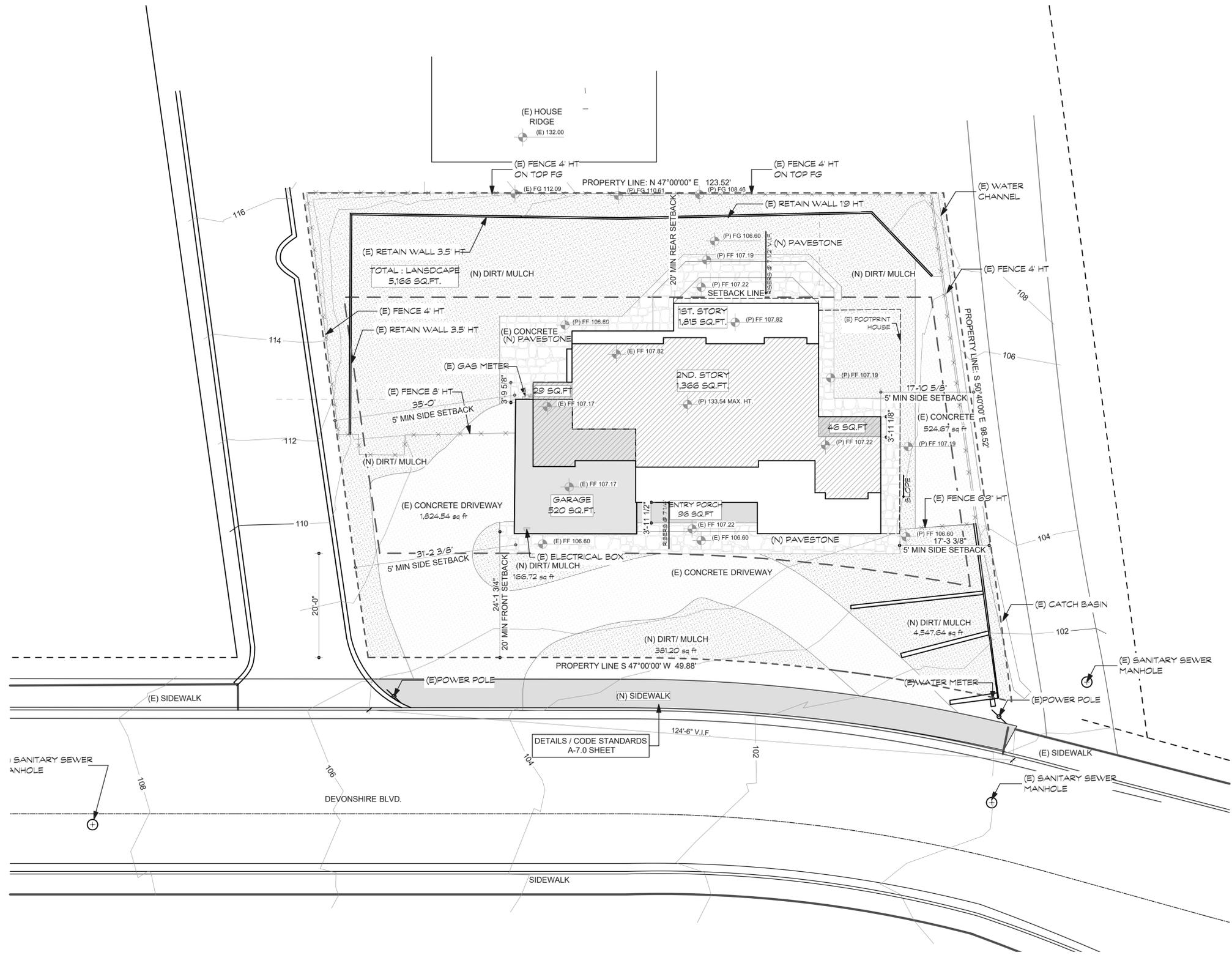
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 2,506 / 11,195 22%

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 3,301 SQ FT
 3,301 / 11,195 29.50%

SITE LEGEND

- DEMOLISH
- REMODEL HOME
- 2ND FLOOR OVERHANGS
- ADDITION
- (E) GARAGE
- (E) FENCE
- PROPERTY LINE
- SETBACK

LANDSCAPE AREAS CALCULATIONS	
ID	SQ. FT.
(H) CONCRETE	48.20
(H) CONCRETE	524.67
(H) CONCRETE	1,800.84
(H) CONCRETE	1,824.54
	4,198.25 sq ft
(H) DIRT	166.72
(H) DIRT	381.20
(H) DIRT	4,547.64
	5,095.56 sq ft
(H) PAVESTONE	112.84
(H) PAVESTONE	311.42
(H) PAVESTONE	837.14
	1,261.40 sq ft
	10,555.21 sq ft

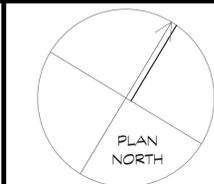


(P) SITE PLAN 1
 SCALE: 1" = 10'



TONG
 RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
 SAN MATEO COUNTY 94070



(P) SITE PLAN

A2.0

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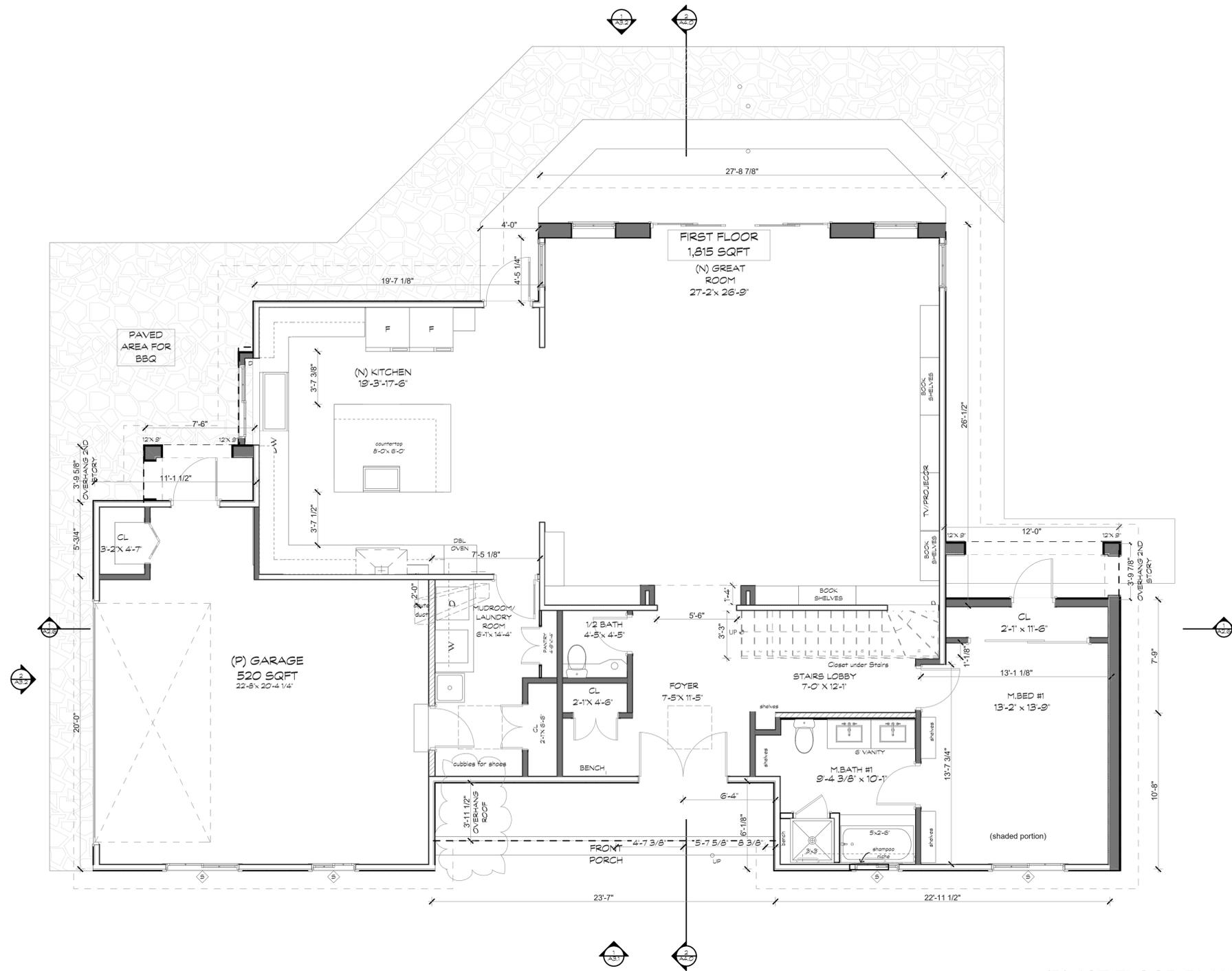
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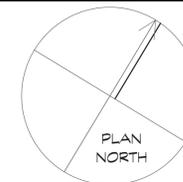


(P) 1ST FLOOR PLAN
 SCALE: 1/4" = 1'-0" 1.



TONG
 RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
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(P) 1ST STORY
 PLAN

A2.1

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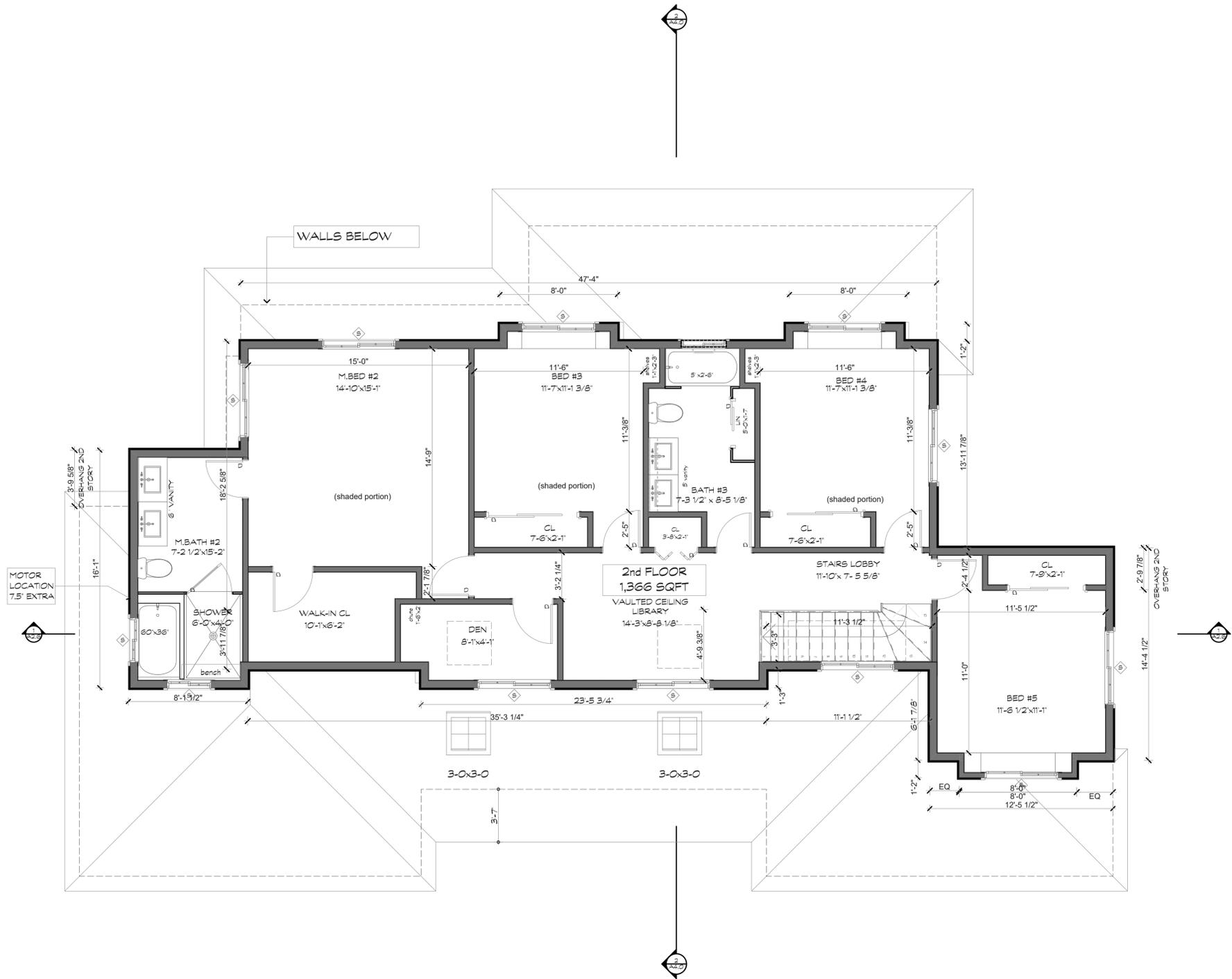
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(P) 2ND FLOOR PLAN

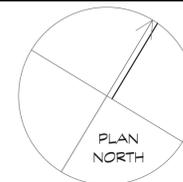
SCALE: 1/4" = 1'-0"

1



TONG
RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
 SAN MATEO COUNTY 94070



(P) 2ND STORY
PLAN

A-2.2

PROJECT DATA TABLE:

APN #: 049-110-160
 ADDRESS: 216 DEVONSHIRE, SAN CARLOS CA.
 ZONING: S-71-DR
 DISTRICT: DEVONSHIRE
 PARCEL SIZE: 11,195 SQ. FT.
 OCCUPANCY: R-1
 TYPE OF CONST: RESIDENCE
 BLOCK ID: 31
 LOT NUMBER: 4
 (E) LANDSCAPING AREA:

(P) LANDSCAPING AREA:
 11,195 -1ST- GARAGE - HARD AREAS
 11,195 - (1,815 + 520 + 3,694) = 5,166 SQ.FT
 TOTAL

ALLOWED BUILDING COVERAGE:
 MAX BSCAR %: 50%
 BSCAR % X LOT AREA
 (0.5 X 11,195) = 5,597.50

ALLOWED BUILDING FAR:
 0.21 (LOT AREA - 5000) + 2000 SQ.FT
 0.21 (11,195 - 5000) + 2000 = 3,301

(E) 1ST FLOOR: 2,305 SQ FT
 (E) GARAGE: 504 SQ FT
 (E) PORCH: 141 SQ FT

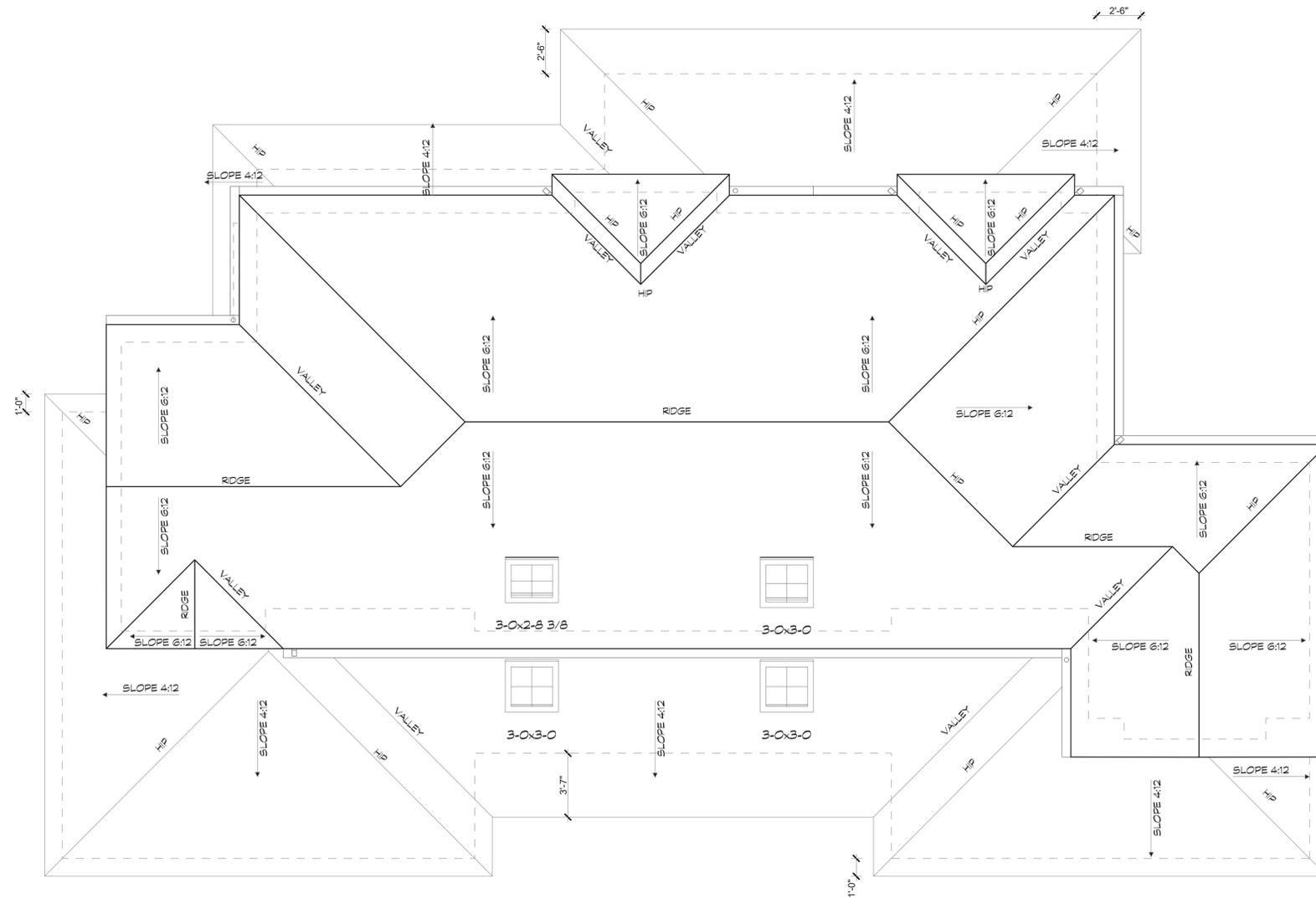
(E) BUILDING COVERAGE:
 2305,504+141 SQ. FT.: 2950 SQ FT

(E) BUILDING FAR:
 2305,104+141 SQ. FT.: 2551 SQ FT

(P) 1ST FLOOR: 1,815 SQ FT
 (P) 2ND FLOOR: 1,366 SQ FT
 (P) GARAGE: 520 SQ FT
 (P) OVERHANG AREAS: 171 SQ FT

(P) BUILDING COVERAGE:
 1,815 + 520 + 171 SQ. FT.: 2,506 SQ FT
 2,506 / 11,195 22%

(P) BUILDING FAR:
 1,815 + 1,366 + (520-400)=120:
 3,301 / 11,195 29.50%

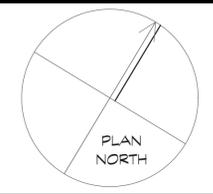


(P) ROOF PLAN 1
 SCALE: 1/4" = 1'-0"



TONG
 RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
 SAN MATEO COUNTY 94070



(P) ROOF PLAN

A2.3

SITE LEGEND

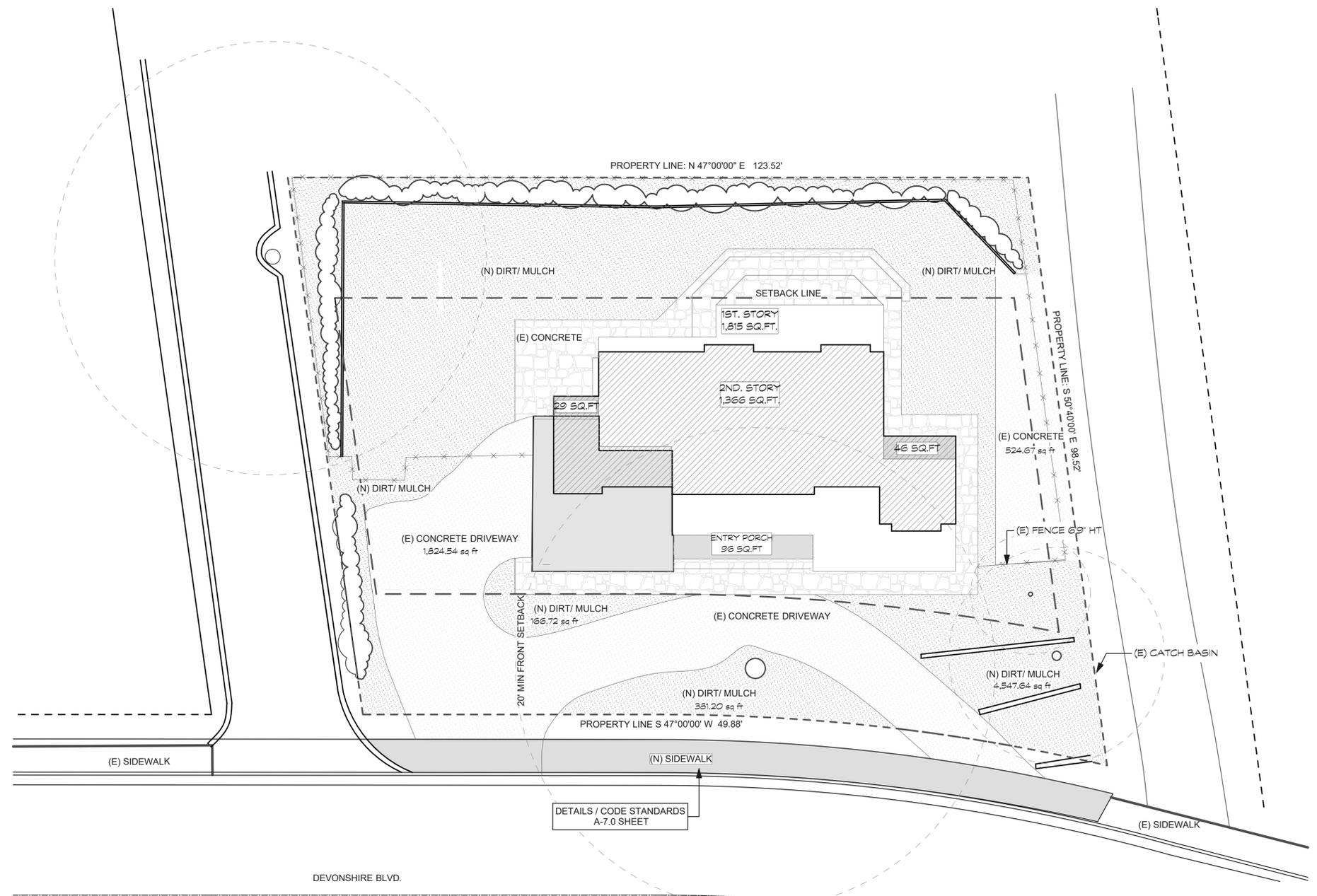
	DEMOLISH
	REMODEL HOME
	2ND FLOOR OVERHANG/EC
	ADDITION
	(E) GARAGE
	(E) FENCE
	PROPERTY LINE
	SETBACK

(P) LANDSCAPING AREA:
 11,195 -1ST- GARAGE - HARD AREAS
 11,195 - (1,815 + 520 + 3,694) = 5,166 SQ.FT
 TOTAL
 ALLOWED BUILDING COVERAGE:
 MAX BSCAR %: 50%
 BSCAR % X LOT AREA
 (0.5 X 11,195) = 5,598

LANDSCAPE AREAS CALCULATIONS

ID	SQ. FT.
(H) CONCRETE	48.20
(H) CONCRETE	524.67
(H) CONCRETE	1,800.84
(H) CONCRETE	1,824.54
	4,198.25 sq ft
(H) DIRT	166.72
(H) DIRT	381.20
(H) DIRT	4,547.64
	5,095.56 sq ft
(H) PAVESTONE	112.84
(H) PAVESTONE	311.42
(H) PAVESTONE	837.14

GENERAL NOTES:
 EXISTING TREES SHALL NOT BE REPLACED OR REMOVED FROM THE ORIGINAL PLACE AS PART OF THIS PERMIT.

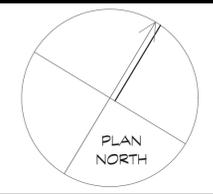


(P) LANDSCAPE PLAN 1
 SCALE: 1" = 10'



TONG
 RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
 SAN MATEO COUNTY 94070



(P) LANDSCAPE
 PLAN

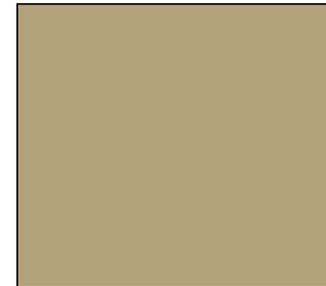
A2.4



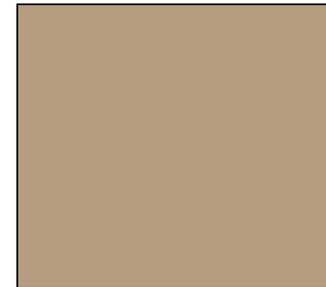
ROOFING: GLENWOOD SHINGLE "ADOBE CLAY" 1



EXTERIOR WALLS: LAHABRA STUCCO MOROCCON SAND 2



EXTERIOR TRIM: DUNN EDWARDS PAINT OLD BOOT DE6133 RL#558 3



EXTERIOR WALL: LAHABRA STUCCO SILVARADO 4



MASONRY BASE: ELDERADO CULTURED STONE "CHARDONNAY COUNTRY" 5

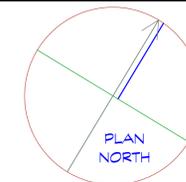


WINDOWS SASH: ANDERSEN WHITE 6



TONG RESIDENCE

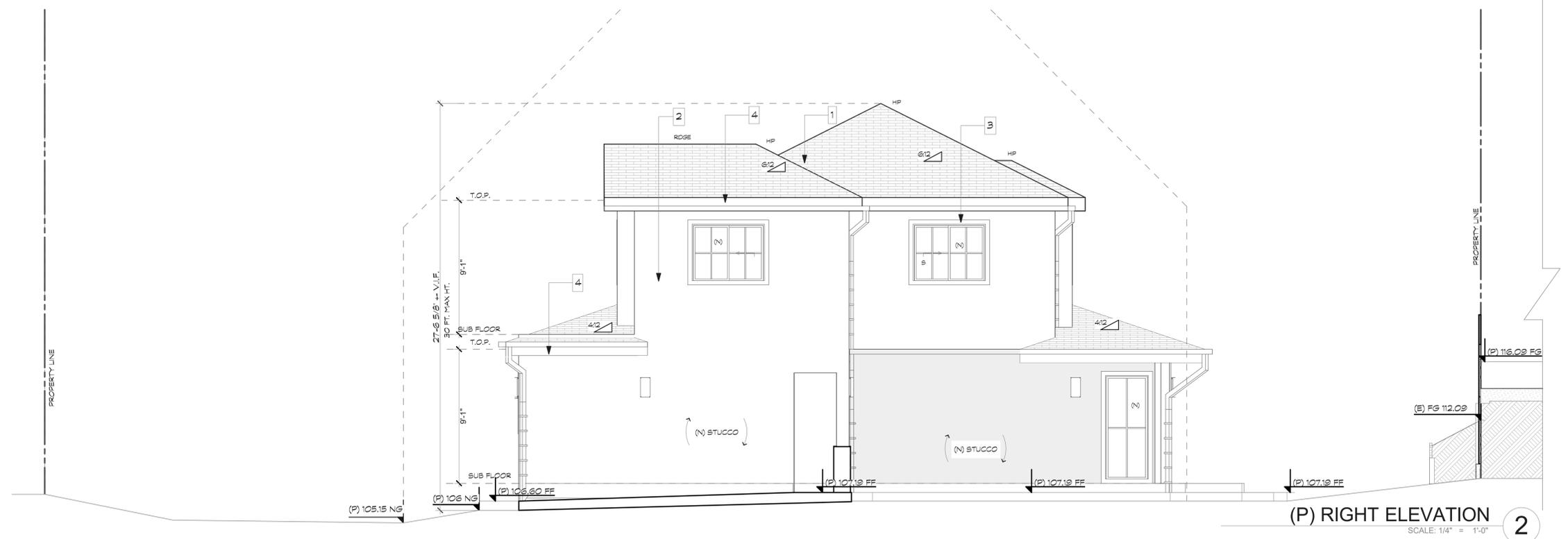
216 DEVONSHIRE BLVD SAN CARLOS, CA
SAN MATEO COUNTY 94070



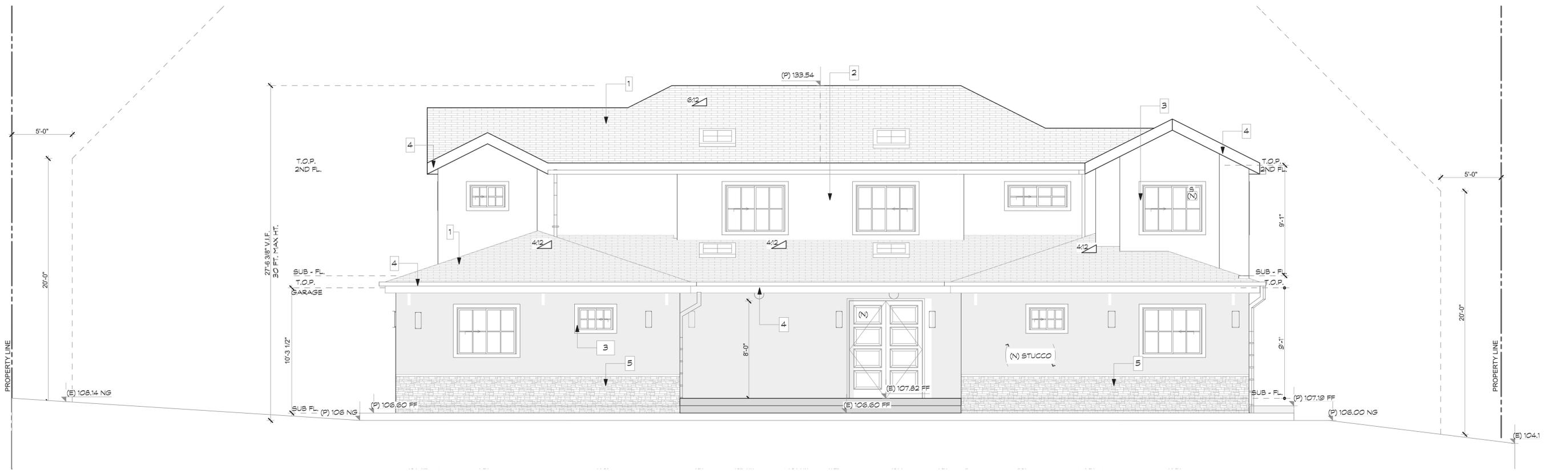
(P) FRONT ELEVATION

A3.0

LEGEND/ NOTES	
	EXISTING WALLS TO REMAIN
	NEW ADDITION
	NEW DOOR/ WINDOWS
	EXISTING DOOR/ WINDOWS



(P) RIGHT ELEVATION 2
SCALE: 1/4" = 1'-0"

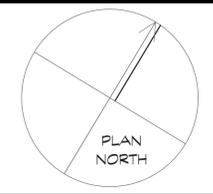


(P) FRONT ELEVATION 1
SCALE: 1/4" = 1'-0"



TONG
RESIDENCE

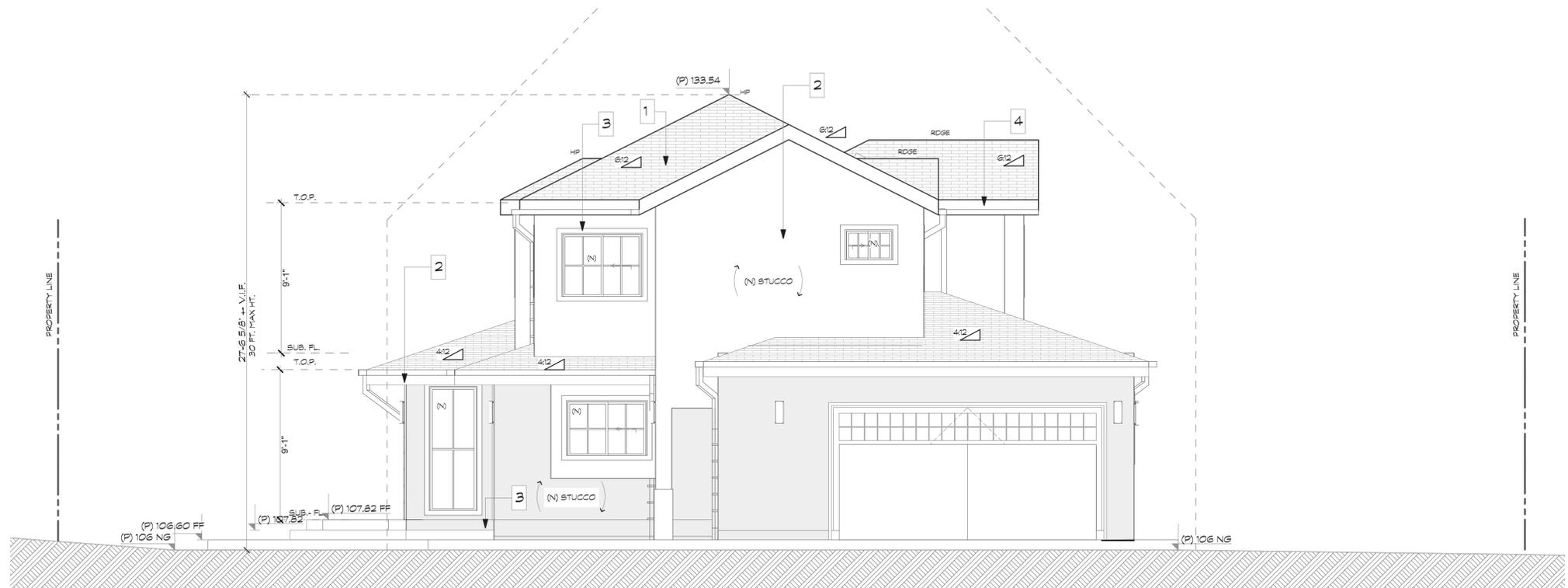
216 DEVONSHIRE BLVD SAN CARLOS, CA
SAN MATEO COUNTY 94070



(P)ELEVATIONS

A3.1

LEGEND/NOTES	
	EXISTING WALLS TO REMAIN
	NEW ADDITION
	NEW DOOR/ WINDOWS
	EXISTING DOOR/ WINDOWS



(P) LEFT ELEVATION
SCALE: 1/4" = 1'-0" 2

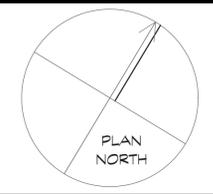


(P) REAR ELEVATION
SCALE: 1/4" = 1'-0" 1



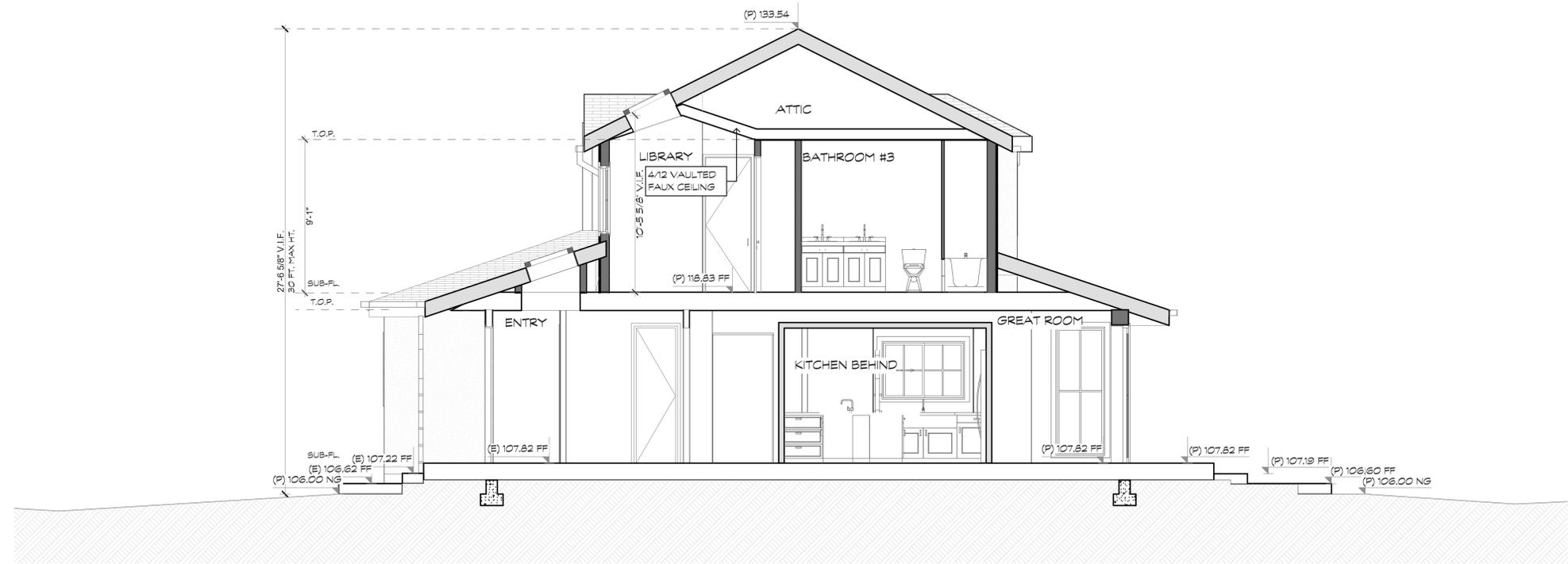
TONG
RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
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(P) ELEVATIONS

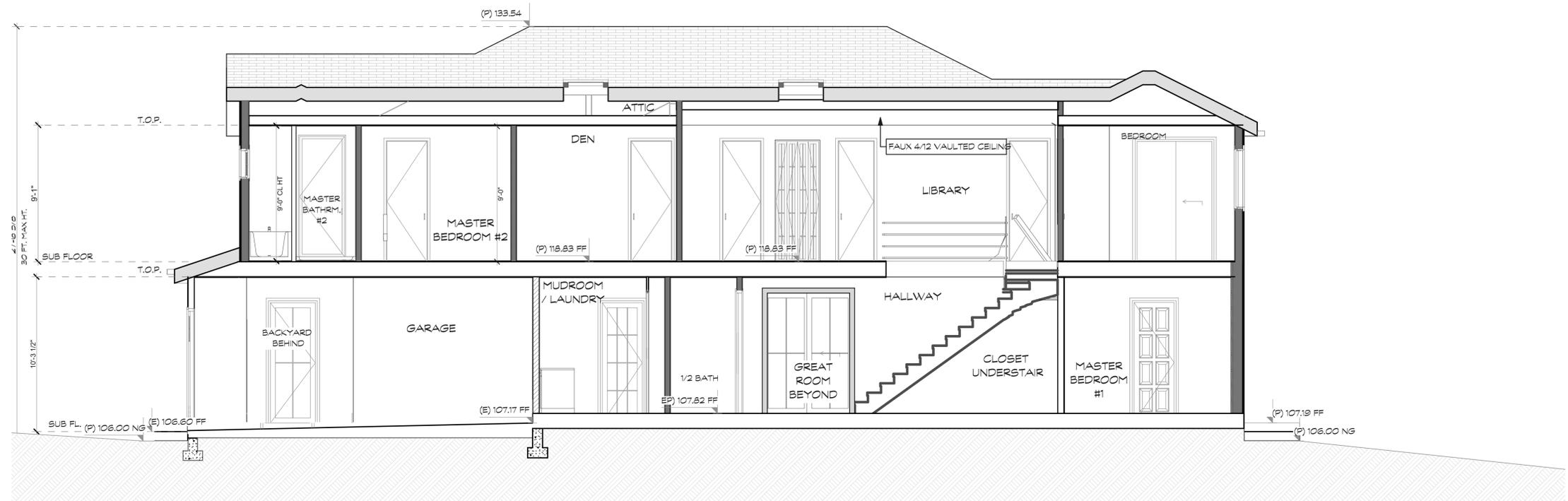
A3.2



SECTION THROUGH LIVING ROOM

SCALE: 1/4" = 1'-0"

2



SECTION THROUGH STAIRS

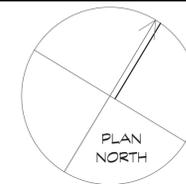
SCALE: 1/4" = 1'-0"

1



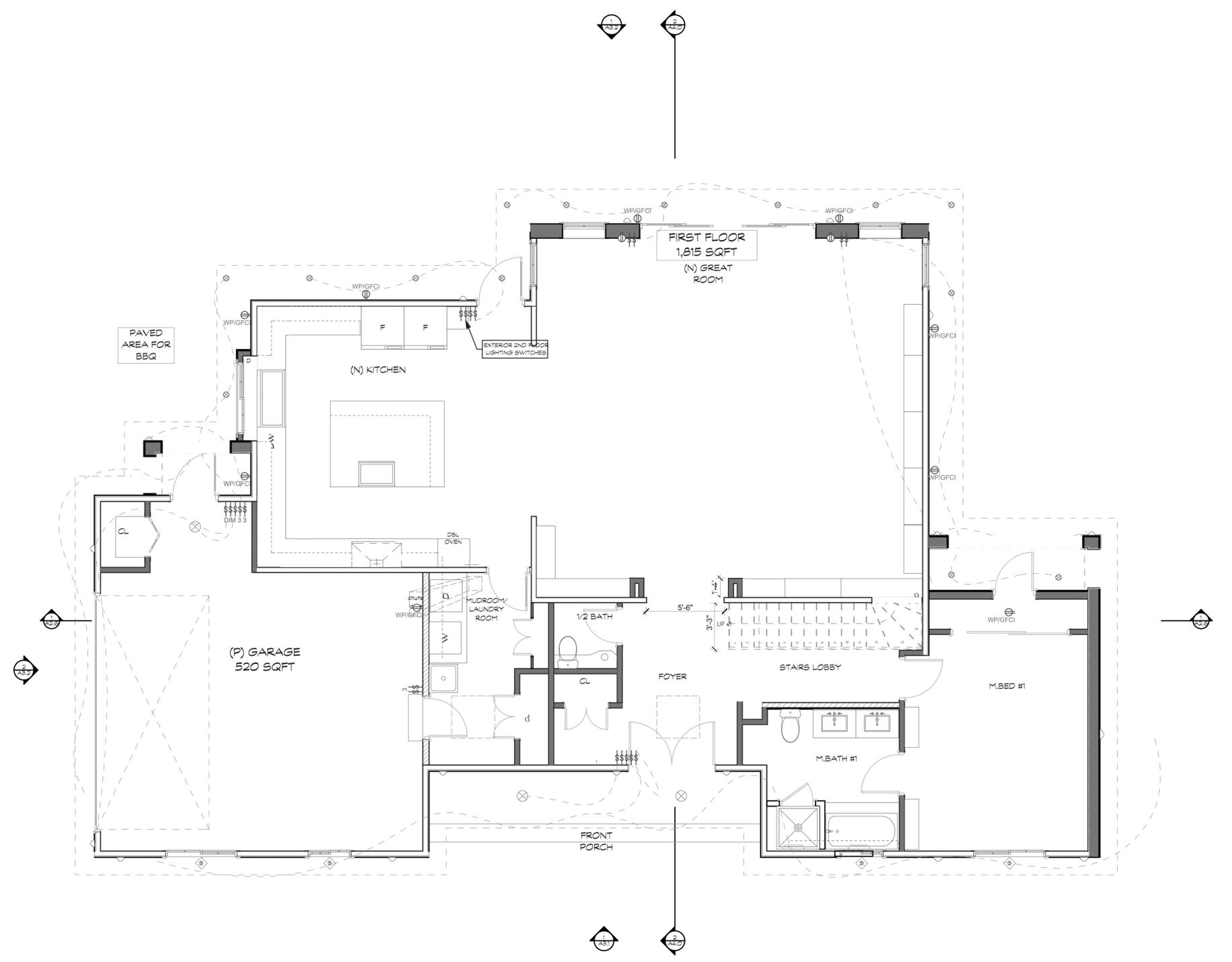
TONG
RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
SAN MATEO COUNTY 94070



(P) SECTIONS

A4.0

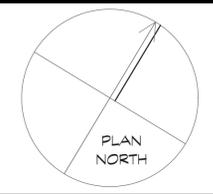


RCP 1ST FLOOR 1.
SCALE: 1/4" = 1'-0"



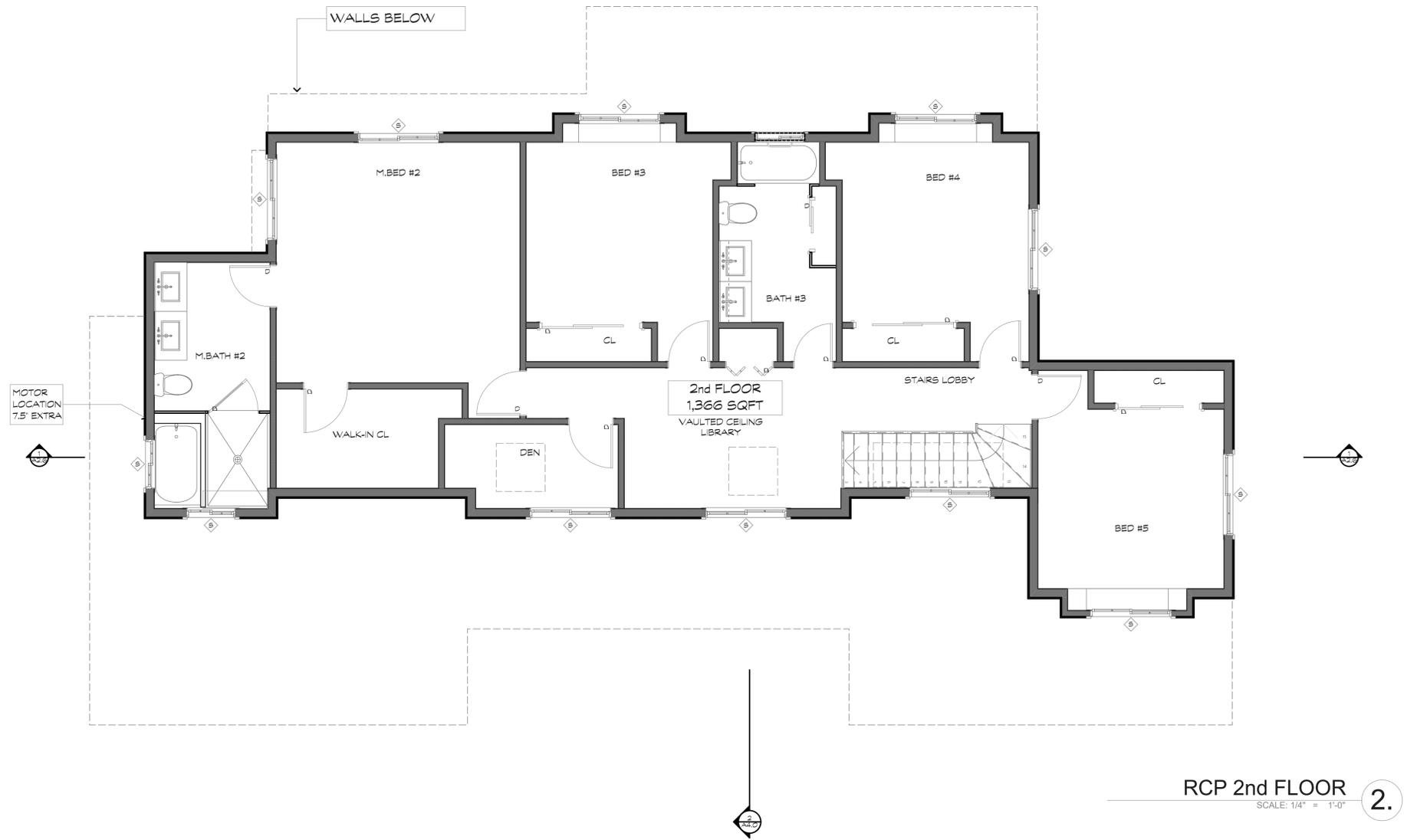
TONG
RESIDENCE

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SAN MATEO COUNTY 94070



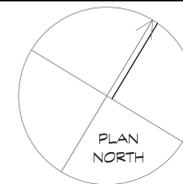
(P) EXTERIOR RCP
1ST STORY

A5.0



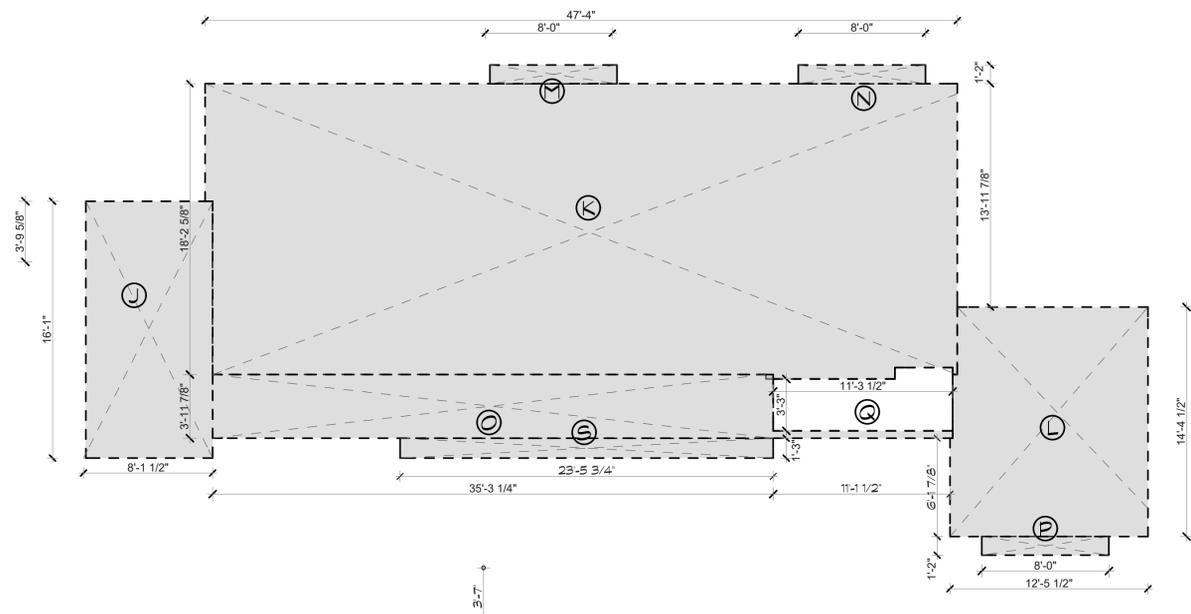
TONG
RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
SAN MATEO COUNTY 94070



(P) RCP 2ND STORY
PLAN

A5.1



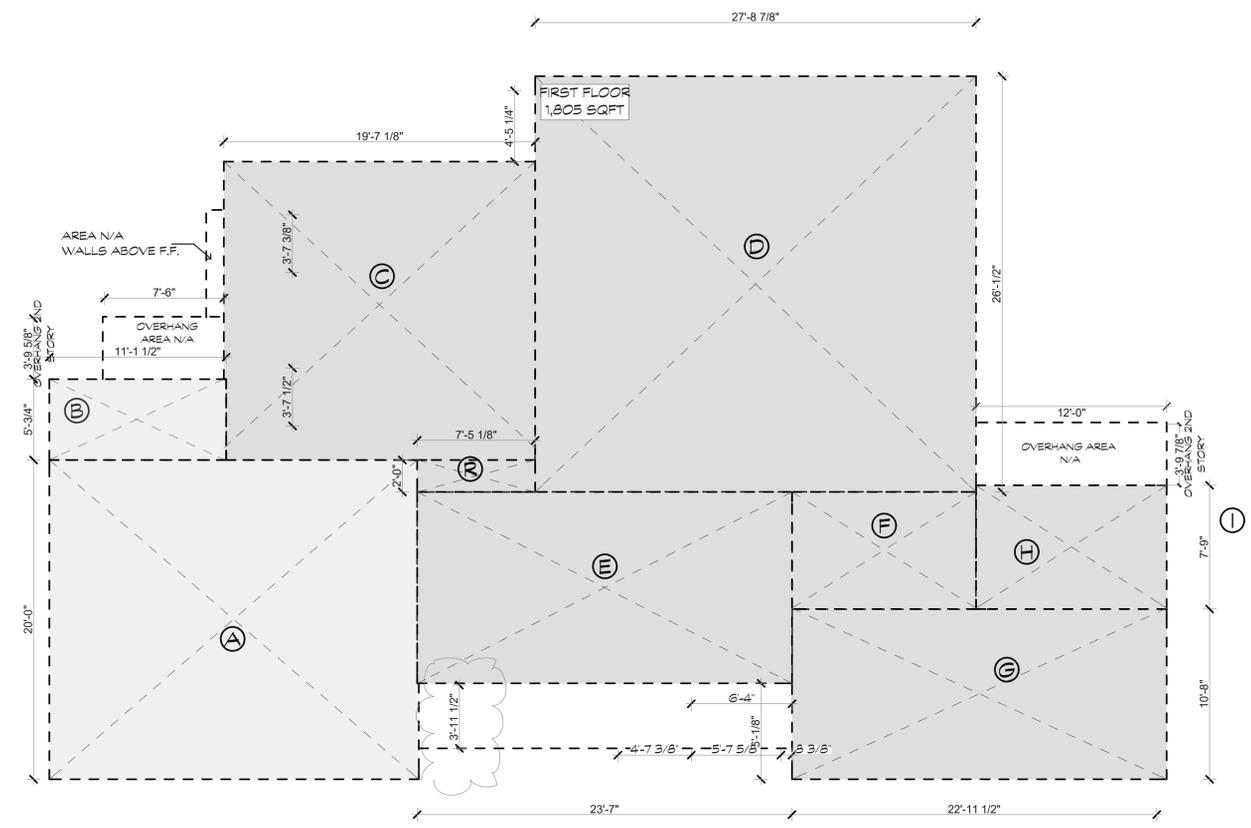
KEY 2ND STORY
SCALE: 3/16" = 1'-0" **2**

PROJECT AREAS:
 (P) GARAGE: A + B = 520 SQ FT
 (P) 1ST FLOOR: C - I + R = 1,815 SQ FT
 (P) 2ND FLOOR: J - P + S = 1,366 SQ FT

KEY AREAS GARAGE	
ID	Area
A	463.57
B	56.33
	519.90 sq ft

KEY AREAS 1ST STORY	
ID	SQ. FT.
C	365.46
D	722.27
E	282.74
F	84.94
G	251.47
H	92.97
R	14.85
	1,814.70 sq ft

KEY AREAS 2ND STORY	
ID	SQ.FT.
J	128.34
K	857.83
L	176.45
M	9.36
N	9.37
O	145.85
P	9.35
S	29.23
	1,365.78 sq ft

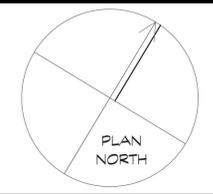


KEY 1ST STORY
SCALE: 3/16" = 1'-0" **1**



TONG
RESIDENCE

216 DEVONSHIRE BLVD SAN CARLOS, CA
SAN MATEO COUNTY 94070



(P) KEY PLANS

A6.0

SPECIAL PROVISIONS -2019

- ACCEPTANCE OF SPECIAL PROVISIONS.** It is understood and agreed by the Permittee that the doing of any work under this permit shall constitute acceptance of the Special provisions.
- NO PRECEDENT ESTABLISHED.** This Permit is granted with the understanding that this action is not to be considered as establishing any precedent on the question of the expediency of permitting any certain kind of encroachment to be erected within right of way of County streets, roads, highways or other areas.
- NOTICE PRIOR TO STARTING WORK.** Before starting work, the Permittee shall notify the Director of Public Works, specifically the Road Division at (650) 363-4103. Such notice shall be given at least 48 hours in advance of the date work is to begin. Any work prior to said notification will be subject to rejection, and/or removal for noncompliance with the notification requirement. The Director of Public Works reserves the right to pass on the responsibility of the contractor who proposes to do the work under this permit.
- KEEP PERMIT ON THE SITE.** This Permit or a copy thereof shall be kept at the site of work and must be shown to any representative of that department or any law enforcement officer on demand.
- PERMIT FROM OTHER AGENCIES.** The party or parties to whom this Permit is issued shall, whenever the same is required by law, secure the permit or consent to any work hereunder from the Public Utilities' Commission of the State of California, to any other public agency having jurisdiction, and this Permit shall be suspended until such permit is obtained.
- PROTECTION OF TRAFFIC.** Adequate provision shall be made for the protection of the traveling public. Barricades shall be placed with warning lights at night and flaggers employed, all as may be required by the particular work in progress. All work shall be planned and carried out so that there will be the least possible inconvenience to the traveling public except for the specific work permitted.
- STOCKPILING OF MATERIAL.** No construction materials or debris shall be stored within eight feet (8') of the edge of pavement or traveled way, or within any shoulder area where the shoulders are wider than eight feet (8'). In no event will any material be stored in a ditch, swale or any other type of watercourse.
- GENERAL CLEAN UP.** Upon completion of the work, all brush, timber, scraps and material shall be entirely removed from the right-of-way and any areas affected by the work shall be left in a presentable condition, and to the satisfaction of the Director of Public Works. Attention is also directed to Section 24, "Water Pollution Control," of this document.
- STANDARDS OF CONSTRUCTION.** All works shall conform to the Standard Specifications of the County of San Mateo, State of California, which are identical with the Standard Specifications, May 2006 of the State of California, Business and Transportation Agency, Department of Transportation, except as set forth in Resolution No. 68389 adopted November 14, 2006. **Special notice is given for the preservation of existing survey monuments.** Where needed, the applicant is advised to get the County Surveyor to tie out the location of existing monument(s) prior to any work. Separate fees for services may apply.
- SUPERVISION BY DIRECTOR OF PUBLIC WORKS.** All the works shall be done subject to the supervision of and to the satisfaction of the Director of Public Works. The Director may appoint an Engineer and/or an Inspector to supervise and approve the work. The terms "Engineer" and "Inspector" shall have the same meaning for purposes of this Permit.
- FUTURE MOVING OF INSTALLATION.** The Permittee is advised that whenever construction, reconstruction, maintenance or any work on the street, highway or other areas may require the removal of the installation permitted herein, the Permittee shall, upon request of the Department, immediately remove the installation at the sole expense of the Permittee.
- EXPENSE OF INSPECTION.** For work that requires the presence of an Inspector or Engineer of the Department, the costs associated with salary, traveling expenses, tests on materials and other incidentals shall be paid by the Permittee. The Permittee may be required to deposit with the Department cash in an amount determined by the Director of Public Works to be sufficient to cover the anticipated costs of inspections. Any remaining balance shall be refunded to the Permittee upon completion and acceptance of the permitted work.
- LIABILITY FOR DAMAGES.** The Permittee is responsible for all liability for personal injury or property damage that may arise out of work herein permitted under this Permit in respect to maintenance. In the event any claim of such liability is made against the County of San Mateo or any Department, officer, or employee thereof, the Permittee shall defend, indemnify and hold each of them harmless from such claim. This Permit shall not be effective for any purpose unless and until the above named Permittee files with the Department, the following Certificates of Insurance: Public Liability in the amount of \$_____ and Property Damage in the amount of \$_____. The Permittee shall carry Workmen's Compensation Insurance to cover all labor employed on work covered by this Permit.
- MAKING REPAIRS.** If the Director of Public Works shall so elect, repairs to paving and other improvements which have been disturbed shall be made by employees of the Department and the expenses therefor shall be borne by the

Permittee. The Director of Public Works shall require a deposit before starting repairs in an amount sufficient to cover the estimated cost thereof. The Director of Public Works will give reasonable notice of his election to make such repairs. If the Director of Public Works does not so elect, the Permittee shall make such repairs in accordance with the Department's construction standards. In every case, the Permittee shall restore any portion of the street, road, highway or other area which has been excavated or otherwise disturbed, to its former condition or to the minimum standards as set forth in these Special Provisions, except where the Director of Public elects to make repairs as provided in this paragraph and except where provision to the contrary is made in this Permit.

- CARE OF DRAINAGE.** If the work item herein contemplated shall interfere with the established drainage, ample provision shall be made by the Permittee to provide for it as may be directed by the Director of Public Works. All storm drainage work shall comply with the provisions of the San Mateo County Drainage Policy and the County's National Pollution Discharge Elimination System (NPDES) permit.
- MAINTENANCE.** The Permittee agrees by the acceptance of this Permit to exercise reasonable care to maintain properly any encroachment placed in the street, road, highway or other areas, and to exercise reasonable care in inspecting for and immediately repairing and making good any injury to any portion of the street, road, highway or other areas which occurs as a result of the maintenance of the encroachment in the street, road, highway or other areas, or as a result of the work done under this Permit, including any and all injury to the street, road, highway or other areas which would have occurred had such work not been done or such encroachment not placed therein.

PIPES, CONDUITS, GAS PUMPS, ETC.

- CROSSING ROADWAY.** On select roads or at locations directed by the Director of Public Works, services and other small diameter pipes shall be jacked or otherwise forced underneath pavement without disturbing same. Service pipes will not be permitted inside of metal culvert pipes used as drainage structures. No tree roots two inches or more in diameter shall be cut. Material shall be removed from around root system so as to avoid damage thereto. Roots shall be protected with burlap wrapping while exposed.
- TUNNELING.** No tunneling will be permitted except on major work as may be specifically set forth on the face hereof.
- DEPTH OF PIPES AND EXCAVATION LIMITS.** There shall be a minimum of two and one-half feet (2 1/2') of cover over all pipes or conduits. The limits of excavation for pipes shall be one-foot (1') minimum outside the outside diameter of the pipe unless otherwise ordered by the Director of Public Works.
- BACKFILLING.** Backfilling operations shall conform to the following requirements. Across roadway sections, curbs, sidewalks, and other paved or surfaced areas, material for use as structure backfill shall have a sand equivalent value of not less than 20. The percentage composition by weight as determined by laboratory sieves shall conform to the following grading:

Sieve Size	Percentage Passing Sieves
3"	100
No. 4	35 - 100
No. 30	20 - 100

Backfill material, properly moistened, shall be placed in horizontal, uniform layers not exceeding 0.67-foot in thickness, before compaction, and shall be brought up uniformly. Each layer of backfill material shall be compacted to a relative compaction of not less than 95 percent.

The backfill material at other locations may consist of material from excavation, free from stones or lumps exceeding 3 inches in greatest dimension, vegetable matter, or other unsatisfactory material and shall be compacted to a relative compaction of not less than 90 percent. When the material from excavation is unsuitable for use as backfill, it shall be disposed of as directed by the Inspector and suitable material approved by the Inspector shall be furnished by the Permittee.

CERTIFICATION OF COMPACTION AND COPIES OF COMPACTION TEST RESULTS SHALL BE SUBMITTED TO THE SAN MATEO COUNTY PUBLIC WORKS DEPARTMENT INSPECTOR FOR REVIEW AND APPROVAL PRIOR TO ACCEPTANCE OF THE WORK BY COUNTY.

THE COUNTY INSPECTOR SHALL DETERMINE THE NEED AND FREQUENCY OF THE TESTING. ALL COSTS ASSOCIATED WITH COMPLYING TO THE ABOVE SHALL BE BORNE BY THE APPLICANT/DEVELOPER OR HIS CONTRACTOR.

- SURFACE MATERIALS.**
 - Aggregate Base:** Mineral aggregate material removed shall be placed with a 3/4" maximum Class 2 Aggregate Base Material, and shall conform to Section 26 of the Standard Specifications of the County of San Mateo. The percentage composition by weight of aggregate base shall conform to the following grading when determined by Test Method No. California 202 modified by Test Method No. California 905 when applicable.

Sieve Size	Individual Test	Percentage Passing Sieves	Moving Average
1"	100	100	
3/4"	87 - 100	90 - 100	
No. 4	30 - 60	35 - 55	
No. 30	5 - 35	10 - 30	
No. 200	0 - 12	2 - 0	

The aggregate base shall conform to the following quality requirement.

Tests	Test Method Northern California	Individual Test	Moving Average
Resistance	301	78 minutes	
<R-Value> Sand	217	28 minutes	31 minutes
Equivalent Durability Index	229	35 minutes	

The depth of aggregate base placed shall be equivalent to the depth of aggregate material removed, unless the existing material is less than 6 inches (6"), in which case, a minimum of 6 inches (6") compacted material will be required.

The aggregate base shall be watered and compacted in layers not to exceed 0.50-foot compacted thickness. The relative compaction of each layer of compacted base material shall not be less than 92 percent of that determined by Test Method No. California 216, for individual tests and not less than 95 percent for moving average.

B. Asphalt Concrete: Surfaced material to be used, to replace all other types of pavements, shall consist of 3/4-inch maximum, medium grade, Type B Asphalt Concrete and shall conform to Section 39 of the Standard Specifications of the County of San Mateo. Minimum thickness of Asphalt Concrete placed shall be 0.17-feet in compacted thickness. Steam-refined paving asphalt to be mixed with aggregate shall be Performance Grade (PG) 70-10 or as directed by the Engineer.

C. Gravel or Crushed Rock Surfaced Roads: Whenever a gravel or crushed rock surfaced street is trenched, the workmanship and materials involved in backfilling and placing surface material shall conform to Section 27 and 22A, of these Special Provisions. During rolling of the final course of the surface material, it shall be sprinkled with water to the extent that sufficient fines will be flushed to the surface to fill the voids and create a seal surface.

D. Moratorium - 2 years on Road recently surface treated by the county: If the Contractor causes any portion of a County road to be excavated and if Contractor's work occurs within two years of the filing of the Notice of Completion for a County street surfacing project, including but not limited to slurry seal, chip seal, resurfacing or road reconstruction, Contractor shall slurry seal the affected County road from edge to edge.

- PIPES ALONG ROADWAY.** Pipes and utilities paralleling the pavement shall be located at the distance from traveled way and at such depth as specifically directed on the face hereof. No tree roots two inches or more in diameter shall be cut. Material shall be removed from around root system so as to avoid damage thereto. Roots shall be protected with burlap wrapping while exposed.

saw-cut with NO exception. Water from saw cutting operations shall not be allowed to enter into any drainage or watercourse.

24. WATER POLLUTION CONTROL

The Permittee is advised that failure to fully comply with the provisions of this Section, and all requirements listed in any resource agency permits obtained for the project, where applicable, shall constitute substantial non-compliance with the requirements of the Clean Water Act, the National Pollution Discharge Elimination System (NPDES) and this Permit.

Construction sites are common sources of water pollution. Materials and wastes that blow or wash into a storm drain, gutter, or street have a direct impact on local creeks and wetlands. San Francisco Bay and the Pacific Ocean. The Permittee shall be responsible for any environmental damage caused by his operations and those of his contractors or employees.

Water pollution shall be defined as including the introduction of any material, including sediment, trash, or other debris, equipment or vehicles into any watercourse, including creeks, ponds, ditches, storm drain facilities, and any surfaces immediately tributary to those areas, except as specifically authorized by any resource agency permits. Water pollution controls are materials and measures that prevent the introduction of any material to any watercourse. "Water pollution control materials and measures may consist of temporary silt fencing; straw mulch/straw logs; spill cleanup materials; pavement sweepers; sand bags or continuous berms; etc.

Water pollution controls shall be applied, maintained and removed by the Permittee as specified herein and as directed by the Inspector. For construction activities occurring between June 15 and September 15, sufficient quantities of applicable water pollution control materials shall be available at the work site prior to commencing any work. For construction activities occurring between September 15 and June 15, all applicable water pollution control measures shall be installed and all applicable water pollution control materials shall be available at the work site prior to commencing any work.

If the measures being taken by the Permittee are inadequate to control water pollution effectively, the Inspector may direct the Permittee to revise the operations. No further work shall be performed until the water pollution control measures are adequate and, if also required, a revised water pollution control program has been approved. Attention is directed to "Permittee Response" of this Section for additional provisions relating to correction of the Permittee's water pollution control program, and payment

Permittee Response

The Permittee is advised that he may be required to respond to the work site after hours and/or on weekends or holidays to mitigate potential water pollution, soil erosion or sedimentation and/or to repair damaged water pollution controls. Failure to respond within four (4) hours of notification by the Department of Public Works shall constitute substantial non-compliance with these Special Provisions.

Should the County Road Maintenance Division be required to provide any after-hours, weekend or holiday repairs to the Permittee's water pollution controls due to the Permittee's failure to respond, all costs associated with providing that response, including overtime wages, equipment and material costs, shall be deducted from the Permittee's deposit and/or invoiced to Permittee, as applicable. The Permittee shall also be fully responsible for any fines, penalties or mitigations imposed by any regulatory agency caused by his failure to respond, regardless if the County Road Maintenance Division attempts any repairs or pollution prevention work in his absence.

Excavation and Grading

The Permittee shall not commence any excavation, backfilling, grading or stockpiling operations until water pollution control materials have been delivered to the work site. Excavation and grading activities shall be scheduled for dry weather periods. Excavation and grading activities shall not be allowed to commence or continue during periods of rainfall or runoff.

General Housekeeping

The Permittee shall control the amount of runoff entering upon disturbed construction and staging areas, particularly during excavation, to reduce the amount of water pollution controls required. Temporary diversion berms and/or sandbags may be employed to divert runoff from entering upon construction and staging areas as approved by the Inspector.

Paved surfaces shall be dry-swept as necessary to prevent water pollution. If pavement flushing is necessary, silt ponds, gravel socks or other techniques to trap sediment and other pollutants shall be required.

All wastes shall be disposed of properly outside the highway right-of-way and, where applicable, in accordance with the Permittee's approved Construction Waste Management plan.

Stockpiles

All soil and/or rock stockpiles shall be protected against wind, rainfall and runoff at all times. Plastic sheeting may be used to cover soils (including aggregate base), and shall be securely anchored by sandbags or other suitable means. At no time will any stockpiled materials be allowed to erode into any watercourse or onto any roadway or other tributary surface.

Spill Prevention and Response

Fluid spills shall not be hosed down. The Permittee shall use dry cleanup methods (absorbent materials, cat litter, and/or rags) whenever possible. If water must be used, the Permittee will be required to collect the water and spilled fluids and dispose of it as hazardous waste. Spilled fluids shall not be allowed to soak into the ground or enter into any watercourse.

Spilled dry materials shall be swept up immediately. The Permittee shall not wash down or bury any dry spills. Spills on dirt areas shall be removed by digging up and properly

Tire Washing

Should the Permittee's equipment be tracking soil onto a public road, the Inspector shall require a tire-washing swale at the exit from the construction site and/or staging areas. The Inspector shall also require that soil be removed from the traveled way by whatever means necessary to prevent water pollution, dust or any other nuisance to the public.

Roadwork and Paving

The Permittee shall avoid creating excess dust when breaking and/or removing asphalt or concrete. Broken asphalt and/or concrete pieces shall be completely removed from the site as soon as possible, or shall be stored in a separate, secure stockpile protected against wind, rainfall and runoff. Material derived from roadway work shall not be allowed to enter any watercourse, or tributary area.

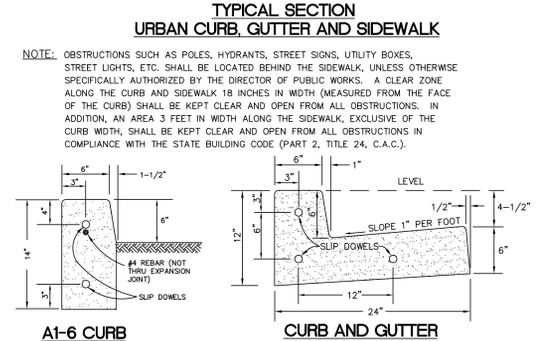
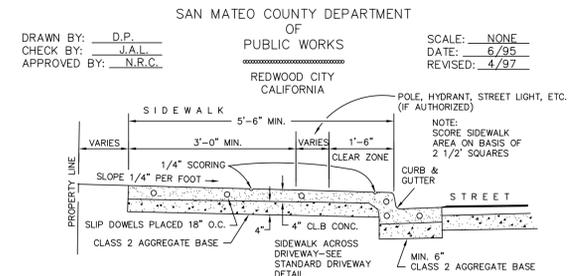
Water/slurry resulting from saw cutting operations shall be shoveled or vacuumed and completely removed from the site. The Permittee shall not be permitted to sweep or flush any saw cutting debris or slurry into any drainage, watercourse, or tributary area.

Concrete and Mortar

The Permittee shall ensure that concrete and mortar are contained within the lines and grades shown on the Plans and not allowed to leave the construction site. Any excess concrete, mortar and/or mix water placed or spilled beyond the limits of concrete construction as shown on the Plans shall be immediately collected, removed and disposed of properly.

No washout of concrete mixers or trucks will be permitted at the project site.

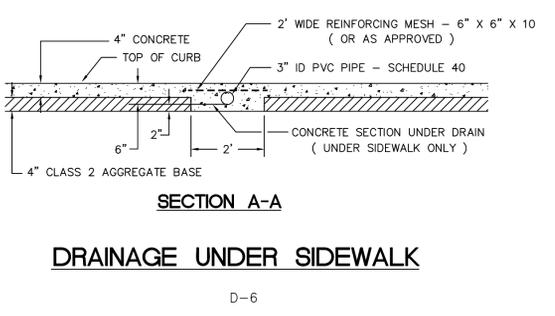
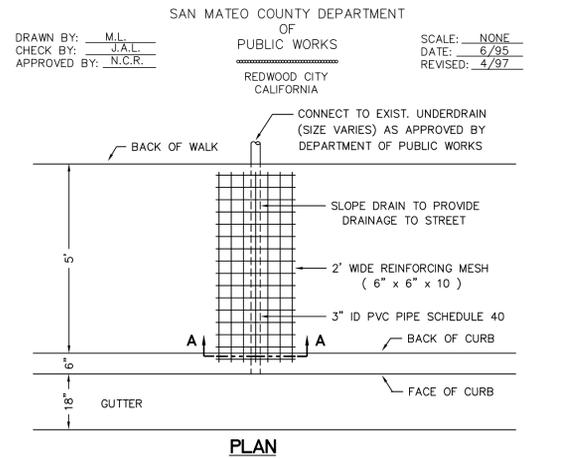
Dry sacks of cement shall be protected against wind, rainfall and runoff. Opened sacks of cement shall be secured and protected from spilling.



- NOTES:**
- ALL CONCRETE TO BE CLASS B CONCRETE.
 - MINIMUM SIDEWALK THICKNESS = 4 INCHES PCC.
 - PLACE 1/2" DIAMETER X 18" LONG DOWELS AT EXPANSION JOINTS AS SHOWN.
 - PLACE 1/2" THICK EXPANSION JOINTS FULL WIDTH 20' ON CENTER.
 - CONSTRUCT MONOLITHIC CURB, GUTTER AND SIDEWALK UNLESS OTHERWISE SPECIFICALLY AUTHORIZED BY THE DIRECTOR OF PUBLIC WORKS.
 - SUB-BASE MATERIALS WITHIN 30" OF SUBGRADE SHALL BE COMPACTED TO 95%. D-3

D3 Curb, Gutter, & Sidewalk Standard 2

NOT TO SCALE



DRAINAGE UNDER SIDEWALK

D-6

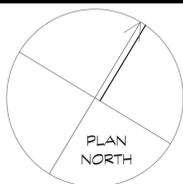
D6 Drainage Under Sidewalk Standard 1

NOT TO SCALE



**TONG
RESIDENCE**

**216 DEVONSHIRE BLVD SAN CARLOS, CA
SAN MATEO COUNTY 94070**



**A-7.0
DETAILS**